

Sketch of the future Balaclava Walk West, looking south towards Nightingale Street

March 2012

City of Port Phillip

Balaclava Walk Masterplan:  
Balaclava Station and Environs

FINAL DRAFT

revised June 2012



Architecture Planning Interior design

Version  
4.2

Purpose  
FINAL DRAFT

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balaclava walk masterplan: balaclava station and environs

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McGauran Giannini Soon Pty Ltd



City of Port Phillip



Introduction

In February 2011, McGauran Giannini Soon Architects Pty Ltd (MGS Architects herein) was engaged by the City of Port Phillip to develop a Masterplan for Balaclava Railway Station and its immediate surrounds. The Masterplan will be part of an integrated vision for the Balaclava Station precinct that comprises the redevelopment of the station, development of the adjoining car park for community and private housing, improved public space and walkway connections (Balaclava Walk in particular), and a high quality interchange between the station and Carlisle Street tram services.

This Masterplan takes into account work previously commissioned by the Victorian Department of Transport for the upgrading of Balaclava Station, and previous work developed for the mixed private and community housing on Marlborough Street.

The project is consistent with the City of Port Phillip’s adopted Carlisle Street Activity Centre Structure Plan, which identifies Balaclava Walk and the station upgrade as two key strategic projects for the centre.

Project objective

To integrate the future design of Balaclava Station with key projects in the surrounding area.

Key projects in the area include:

- > Transforming Carlisle Street into a transport node that caters for passengers using both trains and trams (Precinct One)
- > Developing a mixed community and private housing development on the Council car park to the immediate west of Balaclava Station (Precinct Two)
- > Providing access from Charles Street to Balaclava Station and Balaclava Walk East (Precinct Two)
- > Redeveloping Balaclava Walk East and West; the walkways that run north-south, parallel to the railway embankment between Marlborough and Nightingale Streets (Precinct Three)
- > Creating a southern gateway to the station and Balaclava Walk East and West on Nightingale Street (Precinct Four)
- > Developing Balaclava Walk Southern Link; a walkway running on the western side of the railway line that will connect Balaclava Station with Ripponlea Station (Precinct Five).

Project goals

The Balaclava Walk Masterplan will provide a framework for:

- > A built form and landscape design that will help to realise the vision for the Carlisle Street Activity Centre
- > A coordinated delivery of City of Port Phillip works with the State Government (DOT) delivered upgrade of the station and tram stop
- > An improved functional design of the station and surrounding environs that will benefit shoppers, commuters, local businesses and residents
- > The development of a sustainable transport interchange, with pedestrian movement as the priority form of transport, followed by cycling and public transport
- > An enhanced public realm that is vibrant, safe and attractive in order to encourage increased pedestrian movement
- > A reinforcement of the activity centre’s urban structure and built form elements in order to underpin the identity and legibility of the local area
- > A visual consistency and a sense of streetscape enclosure
- > An enhanced pedestrian experience throughout the precinct
- > A public space adjacent to Balaclava Station that encourages social interaction and improves connections along Balaclava Walk West
- > A higher built form adjacent to Balaclava Station and the rear laneway south of Carlisle Street (recognising these interface areas have the potential to accommodate a more intensive built form).

Project timeline

The integrated Balaclava Walk/station upgrade/community and private housing project was first initiated in 2000 and has actively progressed since 2005. Relevant phases of work include:

- > A Pride of Place funded concept design for the station and Balaclava Walk (2005/06)
- > A feasibility study for the station and community housing (2008)
- > A State Government budget business case for the station redevelopment (May 2010)
- > A review of the Balaclava Walk plan, which identified Balaclava Walk Southern Link as a key project (October 2010)
- > Bipartisan support for a State funded upgrade to Balaclava Station (November 2010)
- > State Government budget allocation to upgrade Balaclava Station (May 2011).

The City of Port Phillip is currently coordinating its broader vision with the Department of Transport (DOT).

The Transport Integration Act (2010)

‘The Transport Integration Act sets out a vision, objectives and principles for transport, making it clear that the transport system needs to be integrated and sustainable - in economic terms, in environmental terms and in social terms - and clearly establishing transport as a triple bottom line issue.

The Act requires all Victorian transport agencies - including the Director of Public Transport, VicRoads, VicTrack, V/Line and the Linking Melbourne Authority – to work together towards the common goal of an integrated and sustainable transport system.

It also means that land use agencies - including the Department of Planning and Community Development, municipal councils [such as the City of Port Phillip], the Growth Areas Authority and Parks Victoria - are required to take account of the new Act when making decisions that impact on the transport system.’

Victorian Department of Transport (2010), Transport Integration Act, <<http://www.transport.vic.gov.au/doi/internet/home.nsf/headingpagesdisplay/about+uslegislationtransport+integration+act>>.

The Transport Integration Act will play a key role in the development of Balaclava Station and its immediate surrounds. As a result of the new Act, the Department of Transport and the City of Port Phillip will need to work in partnership in the design of the station upgrade; assuring the delivery of an integrated and sustainable transport interchange for the people of Balaclava and the wider community.



## 4

Adapted from the Carlisle Street Urban Design Framework, November 2009





## area of study

The masterplanning process will be undertaken in two distinct stages. Stage 1 (comprising Precincts One to Four) is detailed within this report. Precinct Five (the Balaclava Walk Southern Link), will be further addressed in Stage 2 of the Masterplan.

### Area of study

..... Precinct One: Carlisle Street Interface and Transport Node

- 1 > As the gateway to both Balaclava Station and Balaclava Walk, it is proposed that this precinct will provide a high quality civic space for shoppers, commuters and residents alike

Precinct Two: Marlborough Street Housing and Balaclava Station

- 2 > The current station car park will be redeveloped as mixed private and community housing; forming an important interface with the station's new western entry
- > The precinct also covers Balaclava Station's platforms, access ramps and the land to the immediate east

Precinct Three: Balaclava Walk West/Balaclava Walk East

- 3 > The proposed path of two high quality walkways that run either side of the railway embankment, between Marlborough Street and Nightingale Street

Precinct Four: Nightingale Street Interface

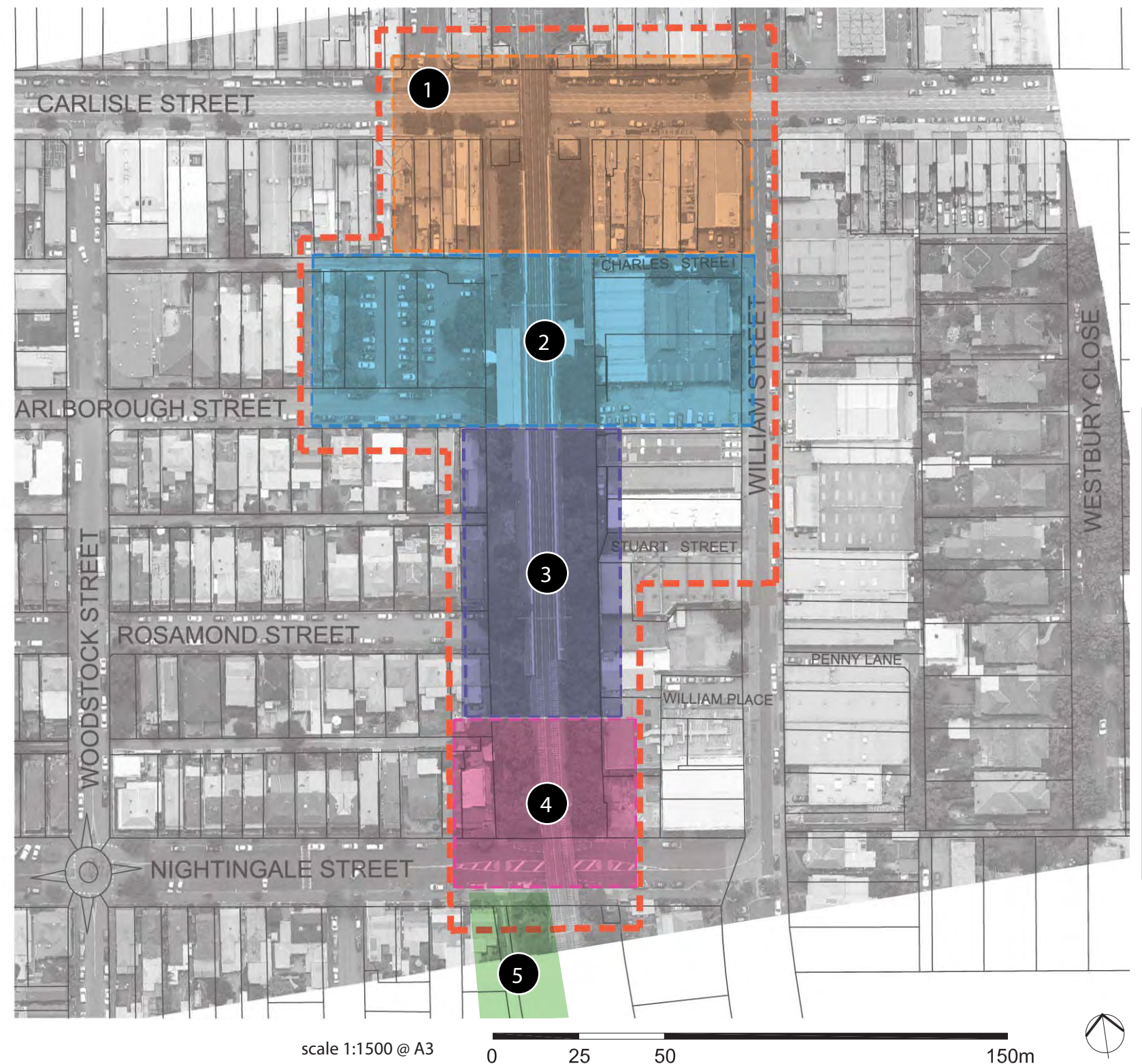
- 4 > A proposed minor civic space that forms the southern gateway to Balaclava Walk East and West

Precinct Five: Balaclava Walk Southern Link

- 5 > A generous and safe walkway that will ultimately link Nightingale Street with Ripponlea Station (Part 2 of the Masterplan)

A large portion of the William Street Industrial Precinct is also included in the study area boundary. This is an important area to consider as it attracts a high volume of

Carlisle Street bound pedestrian traffic. Refer to the Carlisle Street Principal Pedestrian Network Analysis (January 2011) for further detail.





strategic vision

6 Carlisle Street, Balaclava, is designated as a Major Activity Centre in the State Government of Victoria’s metropolitan strategic plan, Melbourne 2030: planning for sustainable growth (2002). The Activity Centre strategy envisages that over the next 30 years these centres will ‘experience considerable change’ and they are expected to achieve:

- > A broader mix of activities (shops, services, and community facilities) that operate over longer hours
- > An increase in residential development and intensity; in and around the centre
- > An increase in public transport patronage, walking and cycling.

The heart of the activity centre is Carlisle Street, the main shopping strip servicing the Balaclava/St Kilda East neighbourhood. Its strong weekly shopping facilities (two major supermarkets), wide range of community facilities and services, and excellent public transport access, means it also draws trade from a wider catchment.

Balaclava Station and its immediate surrounds are located within this activity centre, and as such are affected by the Carlisle Street Activity Centre Structure Plan and Urban Design Framework. Any new development will need to be in line with these municipal documents.

Adapted from the Carlisle Street Activity Centre Structure Plan and Urban Design Framework, November 2009

The vision for the Carlisle Street Activity Centre:

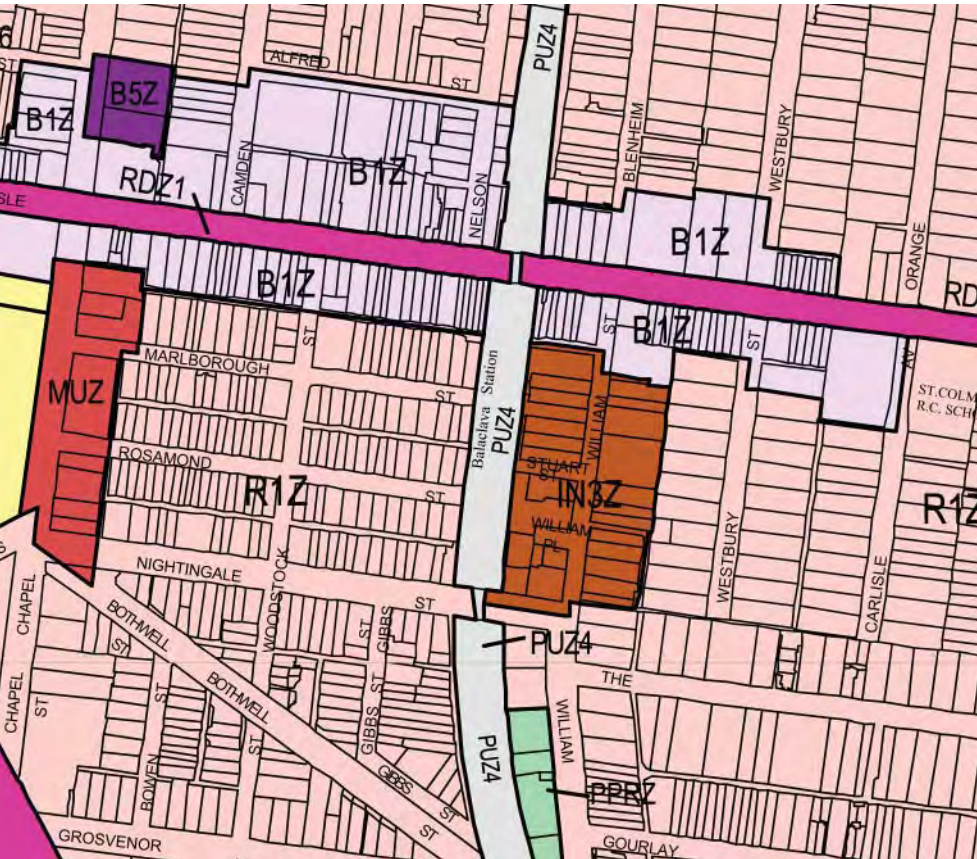
In the year 2020 the Carlisle Street Activity Centre will be enhanced as a focus for the local community, offering a diverse mix of shopping, business and community services, leisure and living opportunities.

Carlisle Street Activity Centre Structure Plan, November 2009





planning context



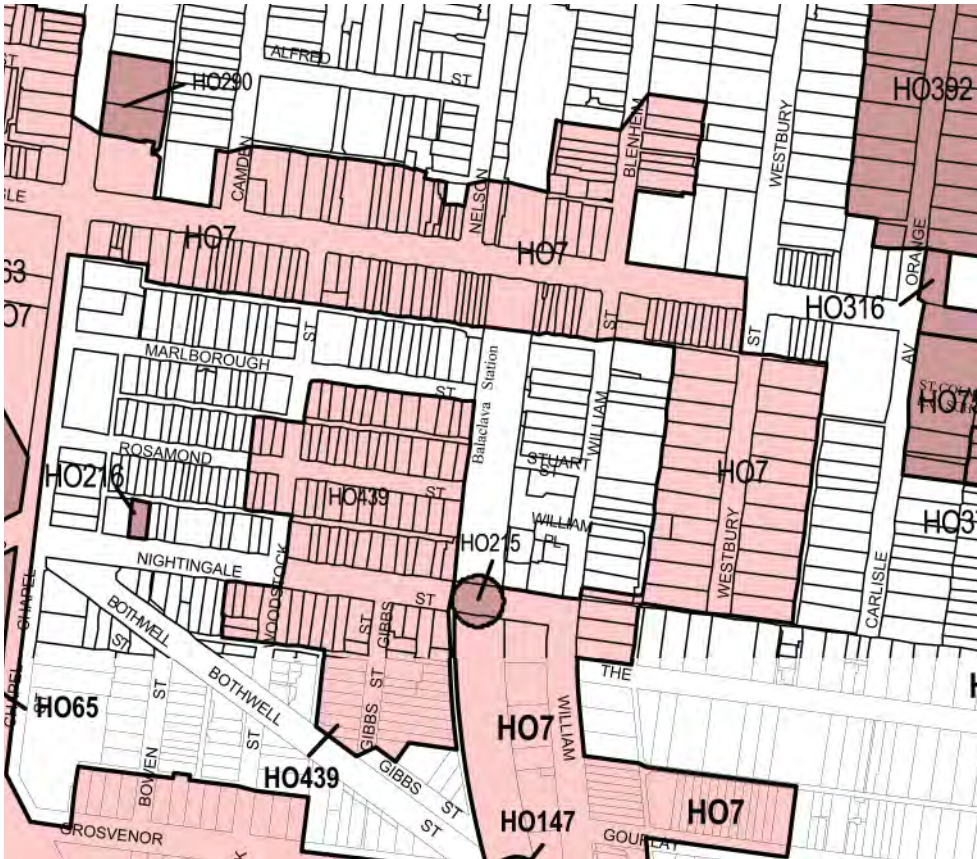
- Zoning**
- Business 1 Zone (B1Z)
  - Business 5 Zone (B5Z)
  - Industrial 3 Zone (IN3Z)
  - Mixed Use Zone (MUZ)
  - Public Park and Recreation Zone (PPRZ)
  - Public Use Zone - Education (PUZ2)
  - Public Use Zone - Transport (PUZ4)
  - Road Zone - Category 1 (RDZ1)
  - Residential 1 Zone (R1Z)

The existing Carlisle Street strip is zoned Business 1 and extends from the west, at the St Kilda Public Library, to the east, at Carlisle Avenue.

An Industrial 3 zone can be found to the south of Carlisle Street along William Street. This is the only remaining area of Industrial zoned land in St Kilda.

The railway reserve, which cuts through the study area is zoned as PUZ4.

The remainder of the study area is largely zoned Residential 1.



- Heritage Overlay**
- Heritage Overlay (HO7)
  - Heritage Overlay - other (HO)

A heritage overlay (HO7) covers the greater portion of the Carlisle Street retail strip with the St Kilda Town Hall, St Kilda Post Office, St Kilda Primary School and Holy Trinity Church Complex protected under site specific heritage overlays and in some cases by a listing on the Victorian Heritage Register.

While the heritage overlays do not preclude development in all cases, they may constrain the extent to which a property either covered by an overlay or adjacent to an overlay may be developed.



- Flood Overlays**
- Special Building Overlay (SBO1) - Melbourne Water gazetted (1 in 100 year flood)
  - Special Building Overlay (SBO2) - City of Port Phillip Planning Scheme flood zone
  - Melbourne Water gazetted sheet flood zone
  - Ungazetted flood zone

Land affected by various flood overlays may be subject to inundation by overland flows from the urban drainage system. Any development in these areas must: maintain the free passage and temporary storage of floodwaters; minimise flood damage; be compatible with the flood hazard and local drainage conditions; and not cause any significant rise in flood level or flow velocity.



## key interventions by precinct

- Balaclava Walk

Pedestrian desire line

Activated frontage

Gateway element

Specific public realm intervention

Bluestone railway bridge abutment improvement works

High quality civic space

Zone of proposed development
- Precinct One:** Carlisle Street Interface and Transport Node

  - > Facilitate the installation of new DDA accessible tram stops on Carlisle Street
  - > Advocate for the removal of structures in front of the southern Bluestone railway bridge abutment
  - > Reinstate and add value to the historic Bluestone railway bridge abutments
  - > Create a northern gateway to Balaclava Walk
  - > Install high quality street furniture, 'real-time' transport information signage, lighting and surface treatments to improve the amenity of this new civic space
  - > Improve connectivity between trains, trams, taxis and 'kiss and ride' facilities
  - > Create a vibrant pedestrian environment
  - > Review the long-term location of the Lady of St. Kilda sculpture whilst recognising its importance.
  - > Increase legibility of the space through the use of an over-arching urban design theme that is common to all precincts.
- Precinct Two:** Marlborough Street Housing and Balaclava Station

  - > Ensure new works align with the final design of the Balaclava Station upgrade, which includes improved passenger access, waiting and ticketing areas and opportunities for small-scale retail
  - > Facilitate and plan for the removal of the at-grade Council car parking and its replacement with mixed community and private housing
  - > Improve the pedestrian environment, including activated frontages to commercial, hospitality and residential uses
  - > Reestablish the fine grain residential character of Marlborough Street at a higher density of development
  - > Encourage the activation of frontages to the southern rear laneway of Carlisle Street
  - > Ensure passive surveillance of Balaclava Walk from the station, community housing and abutting businesses
  - > Reconnect Charles Street to Balaclava Walk East with managed level changes.
- Precinct Three:** Balaclava Walk East and West

  - > Provide new lighting, surface treatments and a thematic approach to Balaclava Walk East and West
  - > Reconnect Stuart Street to Balaclava Walk East with managed level changes
  - > Increase safety and a sense of security by improving sightlines (through path widening at specific points) and installing effective lighting
  - > Introduce pedestrian amenity to both Walks through the use of high quality paving, graphics, signage, street furniture and plantings.
- Precinct Four:** Nightingale Street Interface

  - > Clear the garden beds to expose the heritage Bluestone railway bridge abutments
  - > Tidy up of the environment surrounding the bridge
  - > Facilitate the removal of much of the road signage and traffic islands
  - > Encourage traffic calming in the area to facilitate walking and cycling
  - > Create a minor civic space and gateway to Balaclava Walk Southern Link
  - > Provide an urban design element to announce the gateway significance of the precinct
  - > Facilitate the masking of the electrical substation to the east of the rail bridge.
- The map shows an aerial view of the Balaclava Station area. A central blue-shaded area is labeled 'BALACLAVA STATION UPGRADE ZONE'. To its south is a light blue area labeled 'MARLBOROUGH STREET HOUSING ZONE'. A green line with arrows represents the 'Balaclava Walk' route, running horizontally across the map. Red arrows indicate 'Pedestrian desire lines' along major streets like Carlisle Street, Charles Street, Stuart Street, Marlborough Street, Rosamond Street, and Nightingale Street. Orange dashed lines outline 'High quality civic space' areas. Blue dashed lines outline 'Zone of proposed development' areas. Yellow circles mark 'Specific public realm intervention' points. Blue stars mark 'Gateway element' locations. The map also shows street names: William Street, Charles Street, Stuart Street, Carlisle Street, Marlborough Street, Rosamond Street, and Nightingale Street.
- scale 1:1000 @ A3

0 10 20 30 40 50m
- March 2012
- Balaclava Walk Masterplan: Balaclava Station and Environs
- mgs



## urban design approach

There are four general urban design approaches that the Masterplan is seeking to pursue in the study area. These broad principles will underpin the adopted design solutions in the following section of the Masterplan.

### Enhanced permeability and accessibility to Balaclava Station

- > Strengthen connectivity between Carlisle and Nightingale Street by improving the accessibility of Balaclava Walk East and West
- > Improve connectivity to Balaclava Station from Marlborough and Rosamond Street
- > Improve connectivity to Balaclava Station from William Street, via Charles and Stuart Street
- > Improve connectivity between Balaclava and Ripponlea Stations through the development of Balaclava Walk Southern Link.

### A feeling of safety and security

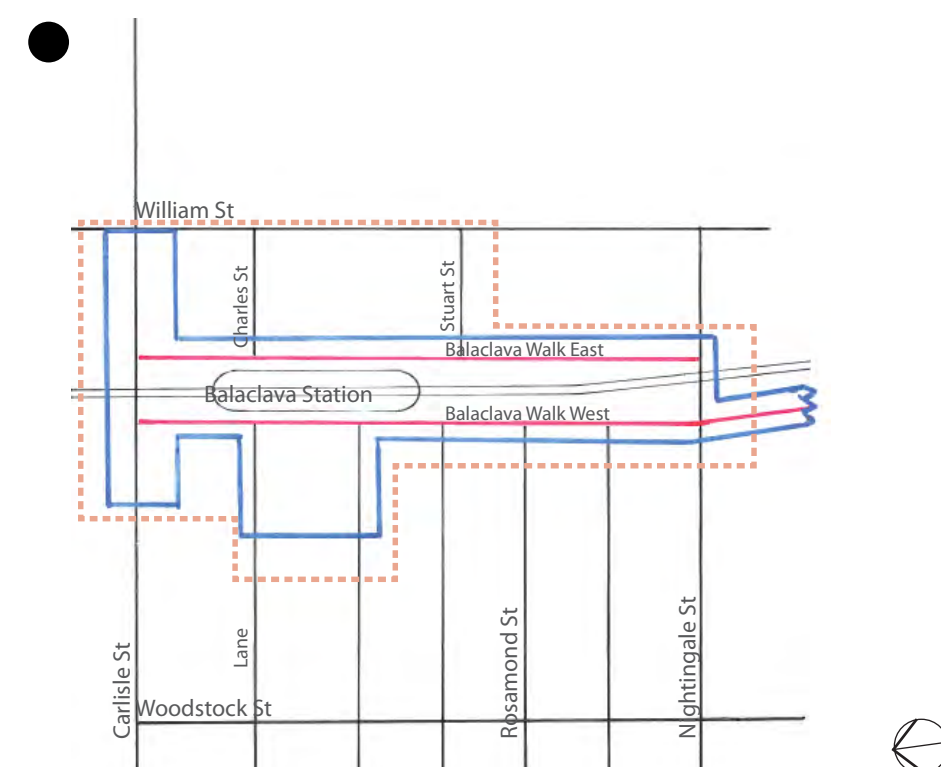
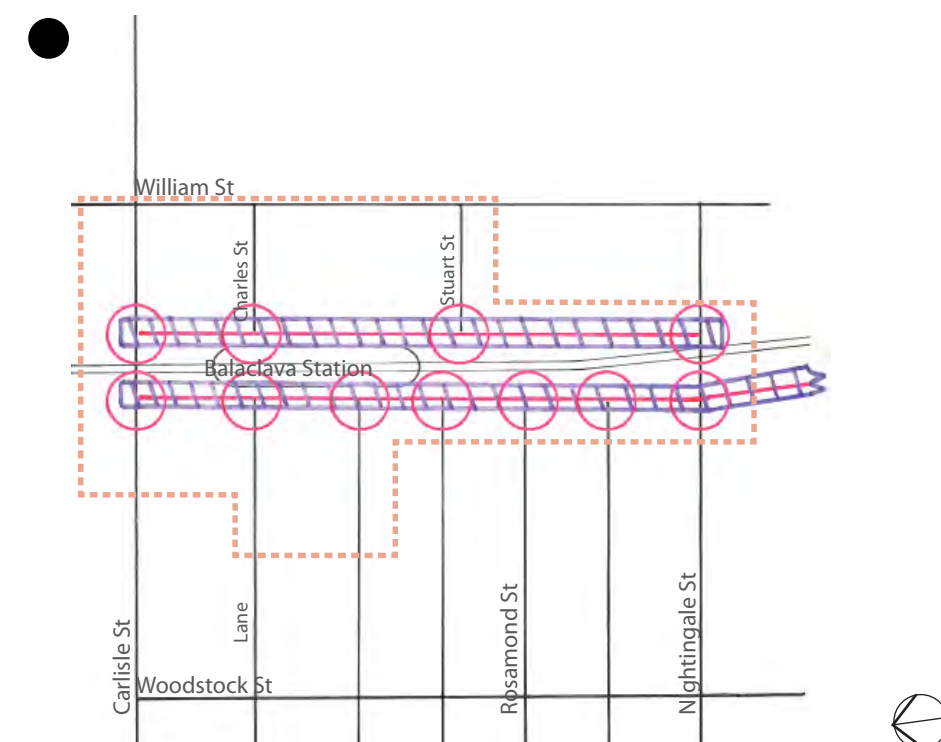
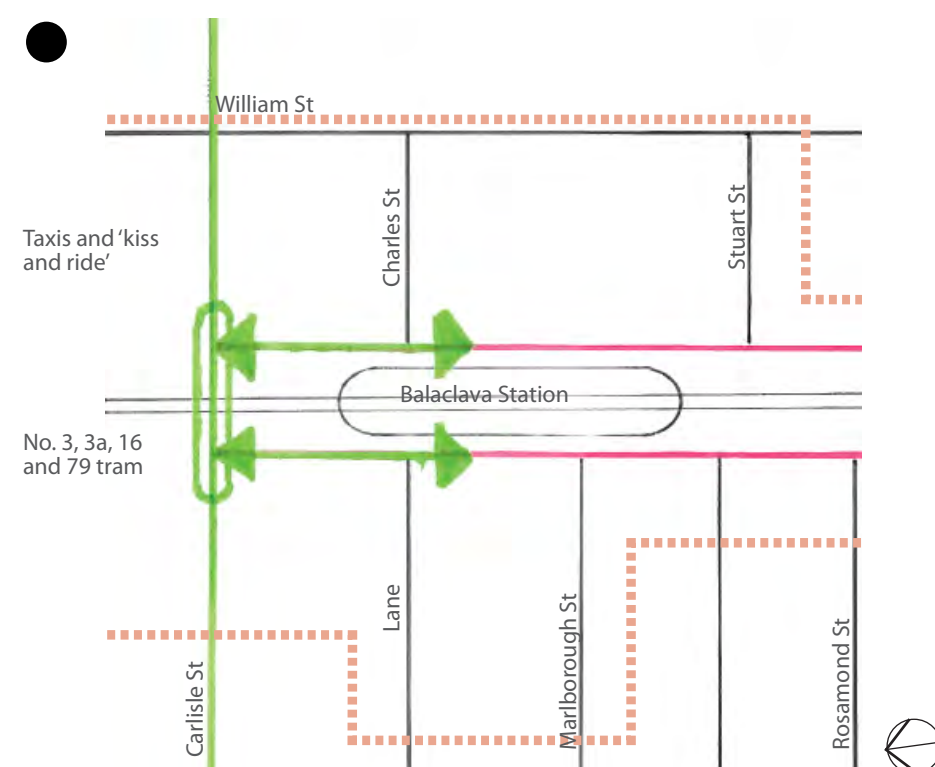
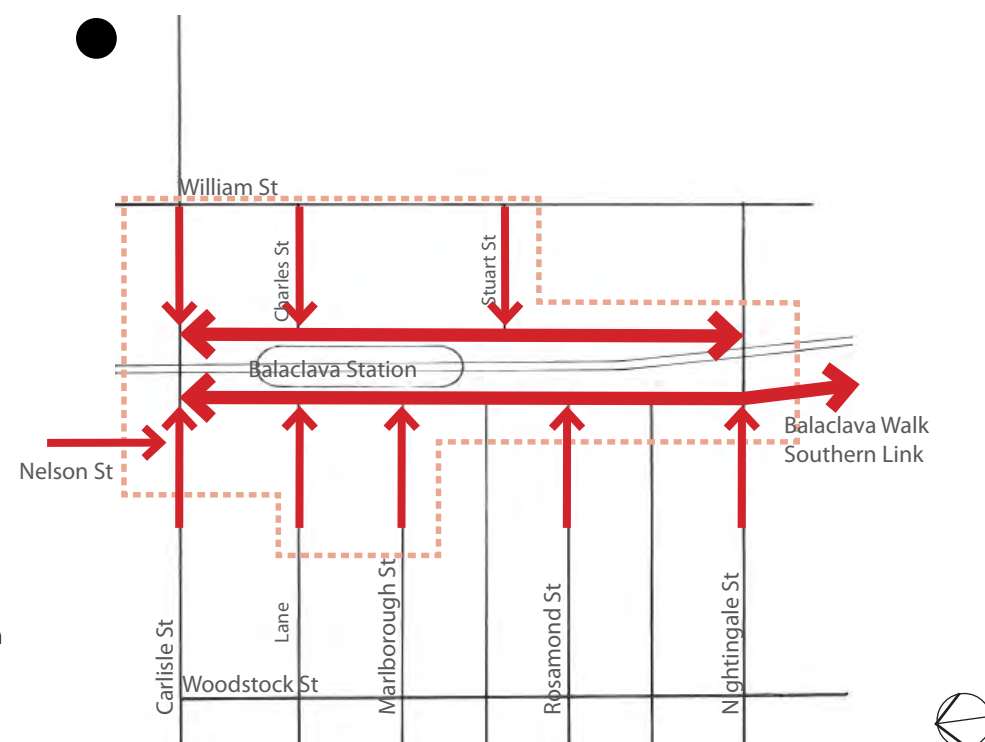
- > Enhance safety and security along both Balaclava Walk East and West through sensitive public realm improvements (eg improved sightlines and lighting)
- > Improve unsafe intersections on both Balaclava Walk East and West.

### Enhanced connectivity between public transport on Carlisle Street and Balaclava Station

- > Provide a safe and efficient pathway/crossing between the Carlisle Street tram stop (no. 3, 3a, 16 and 79 trams) and both Balaclava Station platforms
- > Make improvements to the public realm around the new DDA tram stops
- > Provide easy connections between public transport and waiting taxis
- > Provide efficient 'kiss and ride' facilities for those accessing trams and the station from a private vehicle.

### An improved public realm and consistent surface treatment

- > Ensure that Balaclava Walk and its immediate surrounds are unified by a high quality and consistent surface treatment
- > Concentrate public realm improvements along the length of Balaclava Walk East and West
- > Improve the public realm to the west of Balaclava Station; specifically the interface between the station and the Marlborough Street Housing.





## 4.1

## precinct one:

## carlisle street interface and transport node

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mgs

## Existing Conditions

- > The fine grain retail uses on Carlisle Street create a vibrant and lively environment that attracts a large number of visitors daily (approximately 3,200 commuters per working day)
- > Carlisle Street's thin road reserve width creates conflict between trams, cars, cyclists and pedestrians.
- > The surface treatment throughout the area is not consistent, with patches of concrete and asphalt laid throughout the precinct
- > The quality of the public realm at the station entrance is poor, with little amenity for pedestrians and commuters, including:
  - Tight walkway widths under the rail bridge
  - Limited seating and facilities for public transport users
  - A visually cluttered public realm (signage, light poles, bins, etc)
  - Conflicts between pedestrians, cyclists and cars on the busy street
- > The two existing VicTrack retail tenancies (205-207 and 209 Carlisle Street) contribute to a number of urban design issues, such as:
  - Crowding the entrances to Balaclava Station
  - Reducing views to the station
  - Creating bottlenecks and hindering efficient multi-modal transfers
  - Obscuring the historic Bluestone bridge abutments
- > The Lady of St Kilda sculpture is part of the identity of the street. It is difficult and costly to maintain in its current location. Any future relocation would need to maximise public viewing opportunities.



The interface between Carlisle Street and Balaclava Walk East



The current pedestrian environment below the railway bridge over Carlisle Street



The Bluestone railway bridge abutment on the north side of Carlisle Street



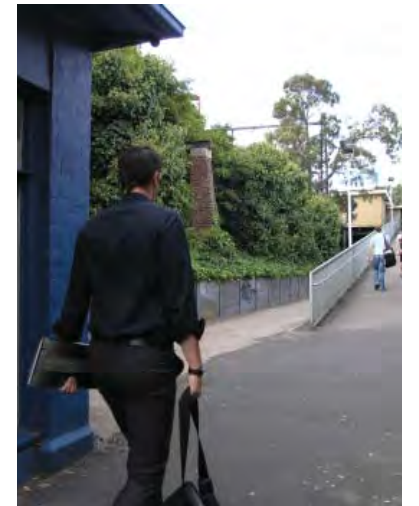
Carlisle Street viewed from Balaclava Walk East



The 'Lady of St Kilda' sculpture attached to the east side of the railway bridge



The VicTrack tenancy located at 209 Carlisle Street; obstructing views of the historic Bluestone railway bridge abutment



The transition from Carlisle Street to Balaclava Walk West





Photomontage of the current Carlisle Street / Balaclava Station interface - looking south east  
The location of the VicTrack tenancy at 205-207 Carlisle Street reduces the presence of Balaclava Station from Carlisle Street



# 4.1

precinct one:

## carlisle street interface and transport node

before

- High tree retention value
- ▭ Advocate for the removal of buildings in front of the southern railway bridge
- Removal of existing station access ramps
- > Rerouting of existing walkway between Carlisle Street and the Marlborough Street car park.



scale 1:250 @ A3

0 5 10 15 20 25m













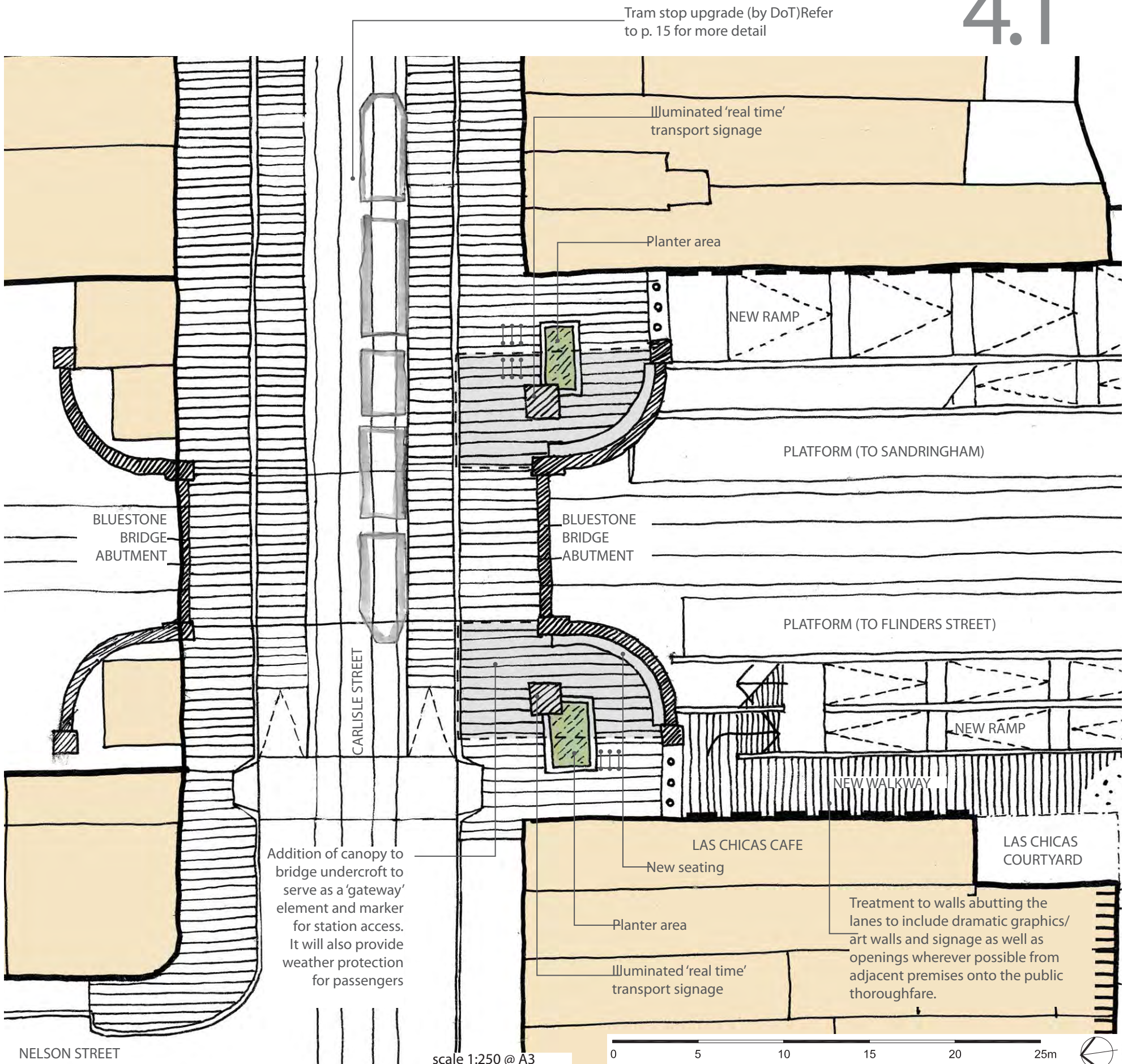
# after

## Design response

- > Advocate to remove the small, VicTrack retail buildings, currently obscuring the curved Bluestone retaining walls on the south side of Carlisle Street
- > Capitalise on the removal of the VicTrack buildings by creating a generous urban space around the southern bridge abutment
- > Develop the precinct as a northern gateway to Balaclava Station and Balaclava Walk East and West
- > Ensure sightlines are clear between Carlisle Street tram stops and the entry to Balaclava Station
- > Transform the precinct into a meeting place and rest area for shoppers and commuters by:
  - Using high quality Bluestone paving as the preferred precinct ground surface treatment
  - Providing seating and feature signage
  - Providing adequate lighting
  - Incorporating appropriate landscaping in the public realm
- > Ensure that the precinct is a comfortable transition area for passengers interchanging between rail and other transport modes on Carlisle Street
- > Provide illuminated, 'real-time' public transport information signage between the tram stops and each station platform access ramp
- > Provide bicycle facilities, such as hoops, close to the station access points
- > Provide vehicle bollards to the north of the platform access points to prevent vehicle through movement
- > Remove redundant or unnecessary street furniture and signage to help reduce visual clutter in the precinct
- > Encourage future activation of the eastern wall of the Las Chicas Cafe.

## Precinct design materials legend

	Existing built form		Graphic design element applied to wall
	Tram stop canopy and gateway element		Bicycle hoops
	Bluestone paving		Bollards
	Ramp (materials to be determined)		Garden bed/small-scale planting





# 4.1

precinct one:

## carlisle street interface and transport node

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mgs



Sketch of the future Carlisle Street/Balaclava Station interface, looking south-east















New tram zone design response

The Department of Transport's (DoT) redevelopment of the tram stops on Carlisle Street will include a number of public realm improvements, such as:

- > Raising the level of Area A\* by 300mm (tram floor level) to enable efficient and accessible tram alighting
- > Providing ramps up to the level of Area A (minimum 5m in length)
- > Extending the pedestrian areas out to the tram tracks
- > Including a bicycle zone running parallel to the tram tracks
- > Providing parking bays close to the platform access ramps
- > Providing 'kiss and ride' spaces for car passengers accessing trains or trams.



Note: this is an indicative design based on previous DoT tram infrastructure improvements. The final tram stop design will be determined by DoT tram standards.

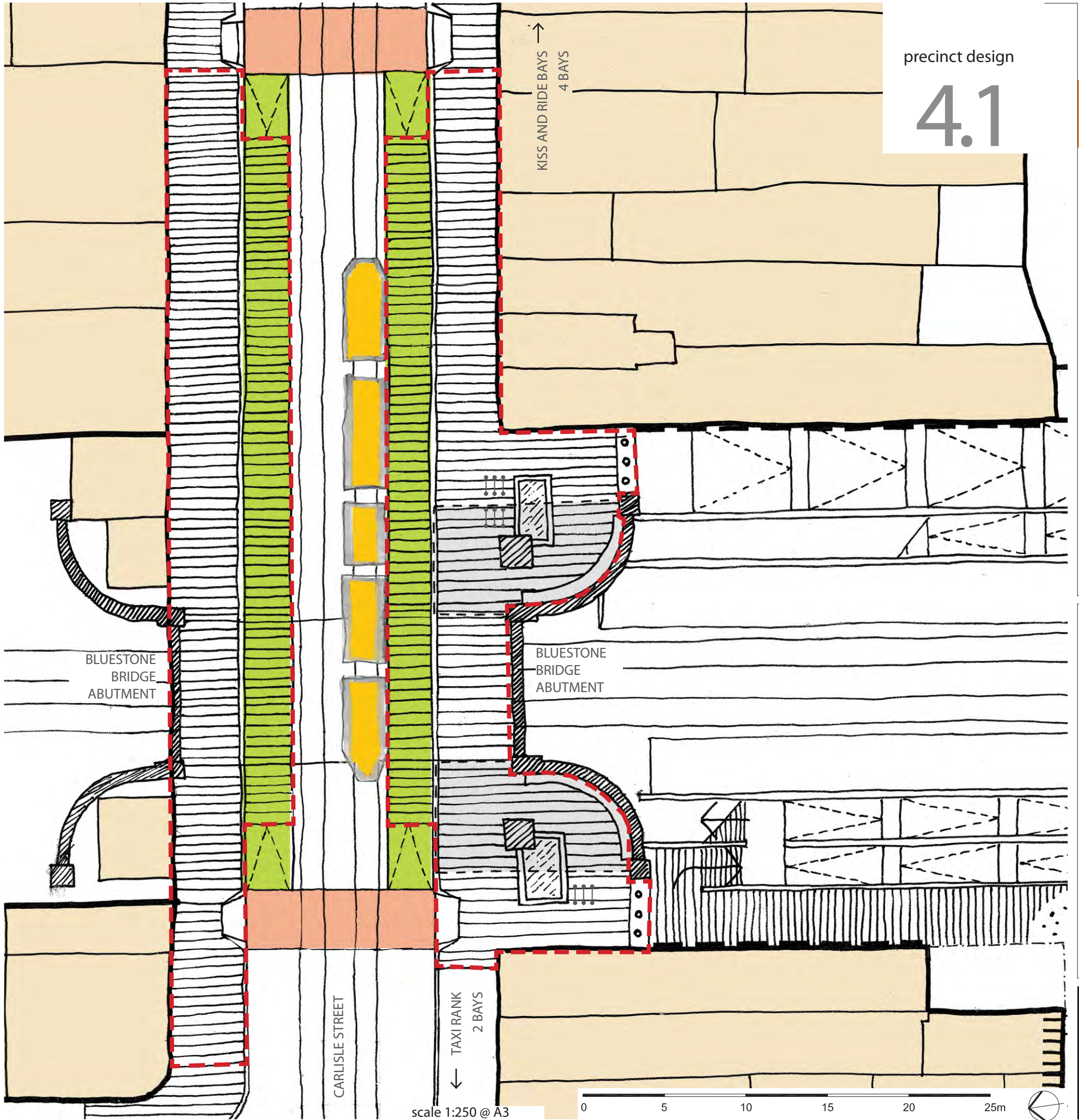
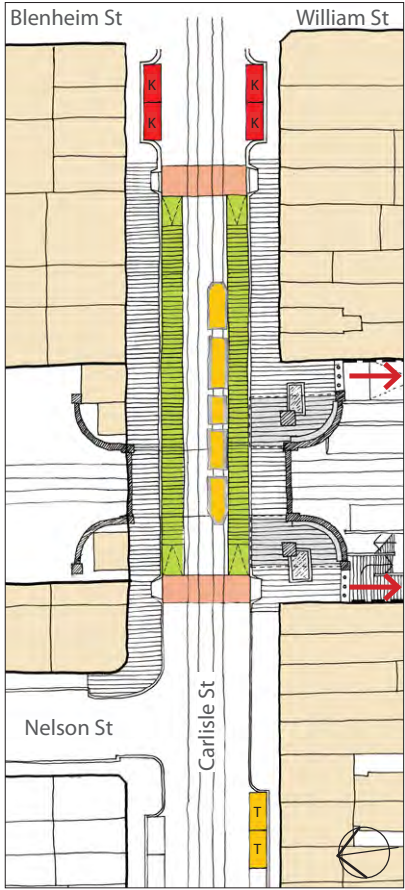
Precinct design materials legend

	Existing built form		Bluestone paving
	Tram stop canopy and gateway element		Ramp (materials to be determined)
	Tram		Graphic design element applied to wall
	Raised bicycle lane/ pedestrian alighting area		Bicycle hoops
	Pedestrian crossing with prominent street graphic		Bollards
	Area A: The level of this area will be raised to that of a low floor tram		Garden bed/small-scale planting

'Kiss and Ride' and taxi spaces

Using the already limited on-street car parking available on Carlisle Street, this design allocates:

-  4 'kiss and ride' (short-term) car spaces, to the east of the Carlisle Street/Balaclava Station interface (approximately 75m from the city-bound platform access ramp)
- >  2 taxi spaces to the west of the Carlisle Street/Balaclava Station interface (approximately 50m from the city-bound platform access ramp).



precinct design

4.1



precinct two:  
marlborough street housing and  
balaclava station

Existing Conditions: Marlborough Street car park

- > The surface of the lane (running parallel and south of Carlisle Street) is in poor condition (this is due for reconstruction by the City of Port Phillip in 2-3 years time)
- > The walkway opening at the eastern end of Marlborough Street (travelling south) is too narrow and does not provide users with adequate visibility; making it dangerous for pedestrians and cyclists
- > The corrugated fence on the railway reserve is an eyesore
- > The railway embankment is unattractive and provides opportunities for antisocial uses
- > Much colour is provided in the area by graffiti, creating an open air gallery; however, some residents and users of the space consider this to be a sign of neglect or find it intimidating
- > The Las Chicas cafe is the only building that has activity fronting onto the car park through the use of its private courtyard
- > Some of the existing plantings in the car park area are in spaces that are too small for the ultimate mature size of the species planted, resulting in infrastructure, such as curb and footpath surfaces, either being damaged or likely to become damaged in the future
- > The northern car park boundary is bordered by unattractive and unactivated back-of-house and servicing uses, which does little to promote passive surveillance and a welcoming public realm.



Unactivated rear lane access to Carlisle Street businesses



The lane running east-west between Carlisle Street businesses and the car park



The Las Chicas cafe; currently the only business using the rear lane as an access point for customers



The western platform access ramp



The Balaclava Station commuter car park; the site for future private and community housing



The walkway bottleneck between Marlborough Street and the continuation of Balaclava Walk West





Photomontage of the current Balaclava Station car park - looking west from the Balaclava Station platform access ramp



precinct two:  
marlborough street housing and  
balaclava station

Existing Conditions: Balaclava Station and access ramps

- > The current Balaclava Walk East pathway is at times too thin and does not provide adequate sightlines for those using the path
- > The walkway surface treatments are in disrepair
- > The design of both station access ramps creates an unsafe pedestrian and cycling environment on the adjacent walkways
- > The access ramps are not DDA compliant
- > The location of the access ramps (set away from the railway embankment) reduces pedestrian sightlines and creates security problems for adjoining businesses
- > The platform and access ramp undercrofts are unattractive and provide opportunities for antisocial behaviour
- > The existing brick station platform supports should be incorporated in the planned station upgrade
- > In some areas the railway embankments are eroded, sparsely planted or infested by weeds
- > Lighting around the station is ad-hoc and insufficient
- > The vista from the top of the eastern station access ramp is blighted by unattractive rooftops and barbed wire fencing.



The eastern station access ramp - looking south



The eastern station access ramp and abutting walkway - looking north



The station platform undercroft is clearly visible and accessible



The current walkway is too thin and in a state of disrepair - looking north to Carlisle Street



The existing brick platform supports should be incorporated into the new station upgrade



The walkway to the car park currently jackknives under the western platform access ramp, creating pedestrian conflicts



The western platform access ramp undercroft is clearly visible and accessible; providing opportunities for antisocial behavior



The western railway embankment is eroded and lacks vegetation





The current city-bound (west) Balaclava Station platform access ramp - looking north to Carlisle Street



The current Sandringham-bound (east) Balaclava Station platform access ramp - looking north to Carlisle Street



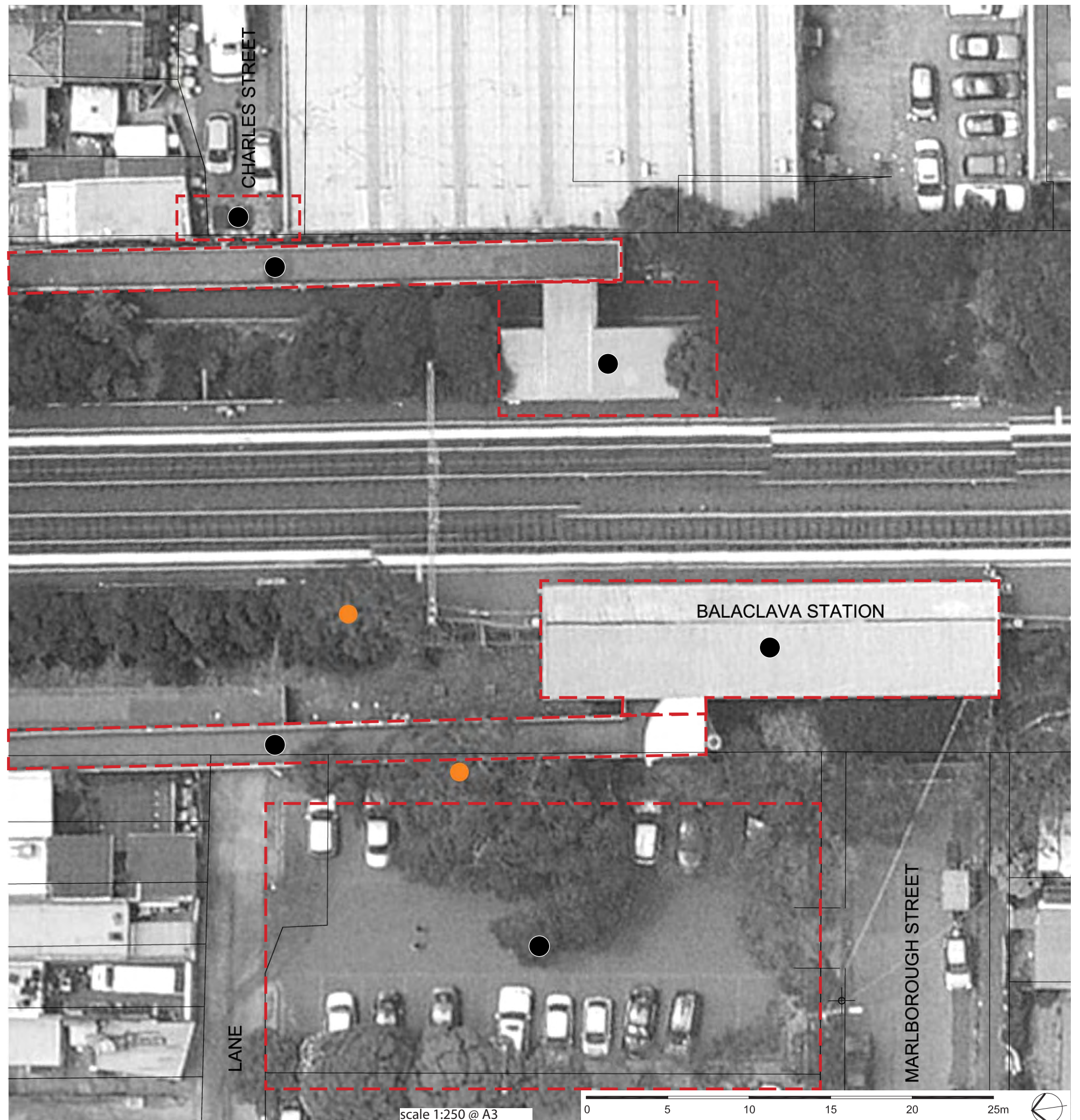
## precinct two:

marlborough street housing and  
balaclava station  
before

● High tree retention value

▤ Areas where change will potentially occur. This will be dependant on DoTs final design for the station.

- Demolition of existing Balaclava Station infrastructure in preparation for the proposed station redevelopment
- Removal of existing station access ramps
- Removal of existing Balaclava Station commuter car park
- Removal of fencing between Charles Street and Balaclava Walk East
- > Rerouting of existing walkway between Carlisle Street and the car park.




















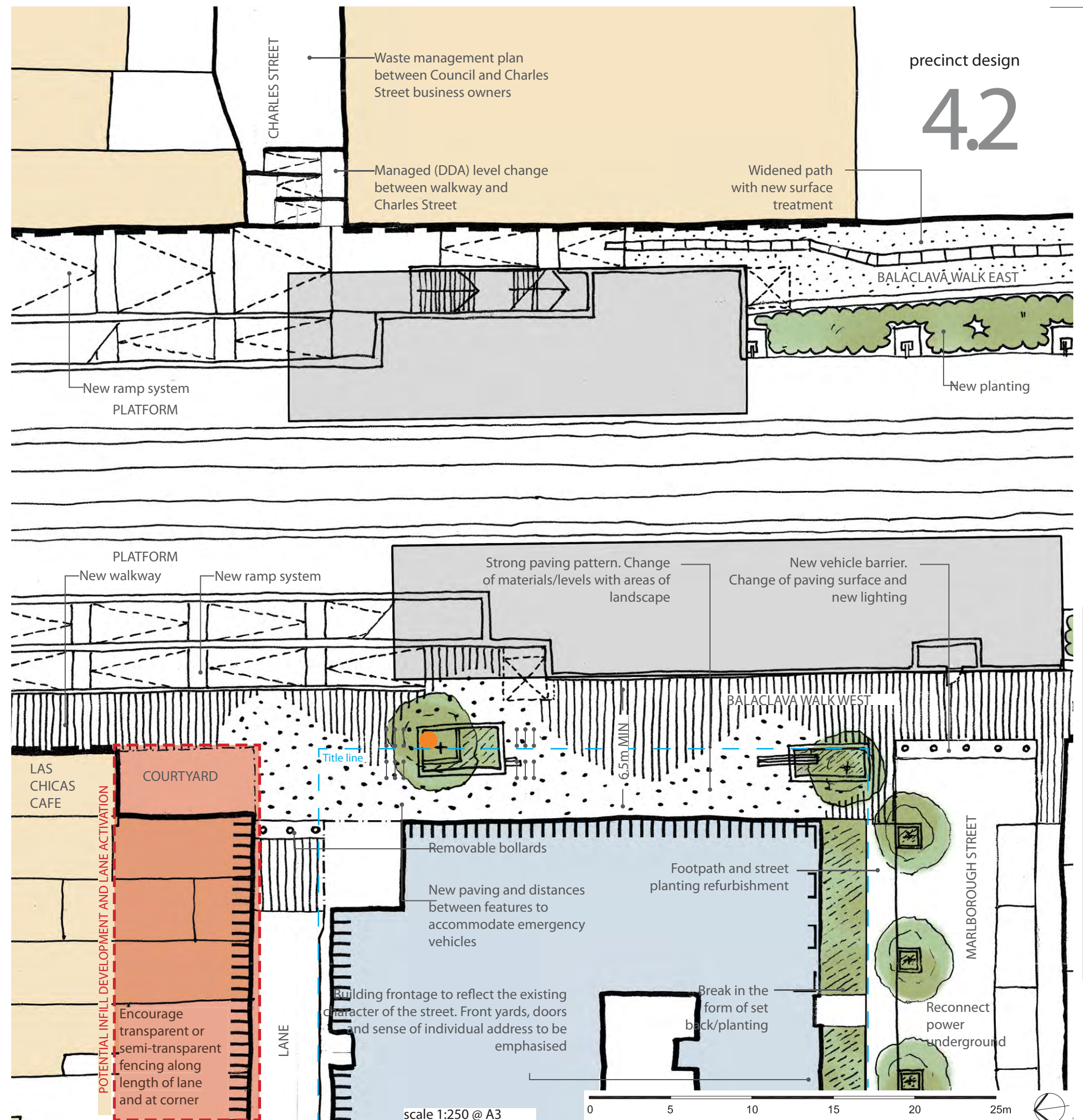
# after

## Design response

- > Facilitate the development of mixed community and private housing on the City of Port Phillip car park
- > Ensure that the housing development contributes to restoring the fine grain residential character of Marlborough Street
- > Ensure that the housing development is setback appropriately in response to smaller existing residential fencelines (approximately 3m)
- > Ensure street trees and landscaping, as well as footpath surface treatments, are a high priority for Council-funded street improvements
- > Encourage new development to accommodate all wiring below ground
- > Ensure that new built form on Marlborough Street mirrors the 'rhythm' of the small workers' cottages that give the street its character
- > Ensure that the housing development provides a sense of multiple addresses along Marlborough Street
- > Create an important civic space between the new station and housing development, which will serve as a high quality pedestrian route from Carlisle Street along the railway embankment. The space will:
  - Develop as a small-scale, yet bright and light space
  - Include new paving and high quality materials
  - Incorporate quality civic design elements, such as street furniture, lighting, striking integrated art, and appropriate landscaping (refer to Section 5)
- > Ensure that the housing is set back from the walkway to create a wider civic space and ensure that it activates its frontage along Balaclava Walk, as well as the lane
- > Invigorate the rear laneway (between Carlisle and Marlborough Streets) and promote it as a legitimate address, rather than just a service-only thoroughfare. This can be achieved with a combination of approaches:
  - Future shop-top housing and infill development of Carlisle Street properties
  - Rear/through access to Carlisle Street retail/hospitality uses and outdoor dining
  - Public realm improvements, including laneway resurfacing, new lighting, alternative waste collection strategies and a security fencing redesign
- > Redesign the pedestrian access to Balaclava Walk West (at the eastern termination of Marlborough Street) to provide a more generous walkway design that maximises sightlines and reduces modal conflicts
- > Ensure that the Marlborough Street - Balaclava Walk West accessway evokes a civic, yet modest, quality
- > Ensure that a vehicle turning area is available at the eastern end of Marlborough Street
- > Provide access to Balacalva Walk East from Charles Street; with level changes negotiated via an accessible ramp and stairs
- > Ensure a healthy pedestrian environment in Charles Street by developing a waste management strategy between Council and Charles Street traders.

## Precinct design materials legend

	Existing built form		Bluestone paving
	Proposed Balaclava Station roof outline		Bluestone paving with strong pavement pattern
	Proposed mixed private and community housing		Asphalt with Bluestone paving strip
	Garden bed/small-scale planting		Ramp (materials to be determined)
	Large-scale planting		Graphic design element applied to wall
	Tree		Activated building frontage to the public realm
	High tree retention value		Bicycle hoops
			Bollards





precinct two:  
marlborough street housing and  
balaclava station



Sketch of the future Balaclava Station mixed private and community housing interface, looking east along Marlborough Street

Sketch of the future connection between Balaclava Walk East and Charles Street, looking west along Charles Street

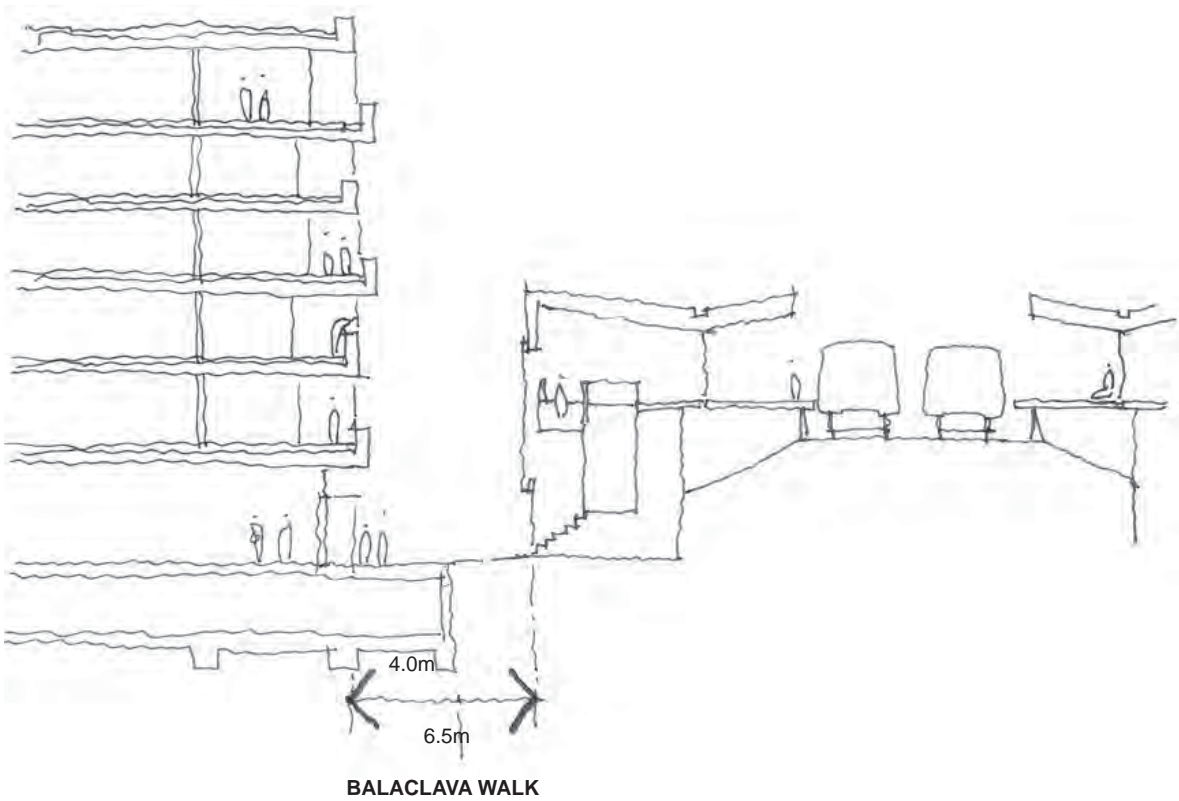




Mixed private and community housing on Marlborough Street: initial concept design, oblique view from the south west. This concept is subject to change and should not be considered to be the final design for the development.

Marlborough Street mixed private and community housing

The above drawings show the concept design for the development. These drawings are preliminary drafts and are subject to change.



Mixed private and community housing on Marlborough Street: initial concept design, section view of the interface with Balaclava Station, looking north to Carlisle Street



precinct three:  
balaclava walk east and west

Existing Conditions: Balaclava Walk East

- > Unattractive and unactivated backs of warehouses front onto the walkway
- > The emergency access route from the southern end of the station platform is breaking up and overgrown with weeds
- > The walkway suffers from heavy shadowing from both the railway embankment and warehousing
- > Abundant tagging in the walkway conveys a sense of neglect
- > The walkway surface treatment is dilapidated and in need of urgent attention (tree roots and general wear have broken the path up)
- > Half of the walkway width is asphalt and the other half is exposed soil
- > Rubbish and debris litter the walkway and it lacks pedestrian amenities, such as seating and lighting
- > Due to a change in level, the walkway width halves and creates a bottleneck
- > The railway embankment appears to have been originally planted (or colonised by) Peppercorn trees, which are a strong and significant landscape element to the station environs and should be retained if possible
- > The railway embankment is overrun by weeds and does not mask the station platform undercroft
- > Rear access to William Street businesses is often separated from Balaclava Walk East by poor quality fencing
- > There is a significant level change between the walkway and the termination of Stuart Street, which will make any future connections a challenge.



Even though the path is in disrepair, it still attracts significant pedestrian traffic



In some areas, Balaclava Walk East is split into asphalt and gravel/soil



An example of the neglected public realm on Balaclava Walk East



The western end of Stuart Street is separated from Balaclava Walk East by fencing and a significant level change (approximately 0.8m)



Rear access to William Street businesses is separated from Balaclava Walk East by poor quality fencing



Balaclava Walk East - viewed from the eastern station platform





Balaclava Walk East - looking north



Balaclava Walk East - looking north



precinct three:  
balaclava walk east and west

Existing Conditions: Balaclava Walk West

- > The walkway has access issues (not DDA compliant) and is too narrow
- > The walkway suffers from a lack of quality lighting at night
- > Plantings are unsuitable for the area; they often overshadow the walkway and provide opportunities for antisocial behaviour
- > Sightlines are poor and sharp corners create pedestrian conflicts (the lack of clear sightlines can also create a sense of unease for pedestrians)
- > Abundant ‘tagging’ in the walkway conveys a sense of neglect
- > The interface between residential uses and the walkway is poorly managed
- > The station platform undercroft is clearly visible and accessible
- > The intersection between the laneway, which runs east-west between Marlborough Street and Rosamond Street and terminates at Balaclava Walk West is neglected and not pedestrian-friendly
- > Rubbish and debris litter the walkway and it lacks pedestrian amenities, such as street furniture
- > The railway embankment appears to have been originally planted (or colonised by) Peppercorn trees, which are a strong and significant landscape element to the station environs and should be retained if possible
- > The railway embankment is overrun by weeds in some areas and is eroded.



The current bottleneck between the eastern end of Marlborough Street and Balaclava Walk West



Balaclava Walk West - looking north



Balaclava Walk West - looking south



Intersection of Balaclava Walk West and the lane running between Marlborough and Rosamond Streets



The transition between Rosamond Street and Balaclava Walk West is poor, with little investment in the public realm





The intersection of Rosamond Street and Balaclava Walk West - looking north

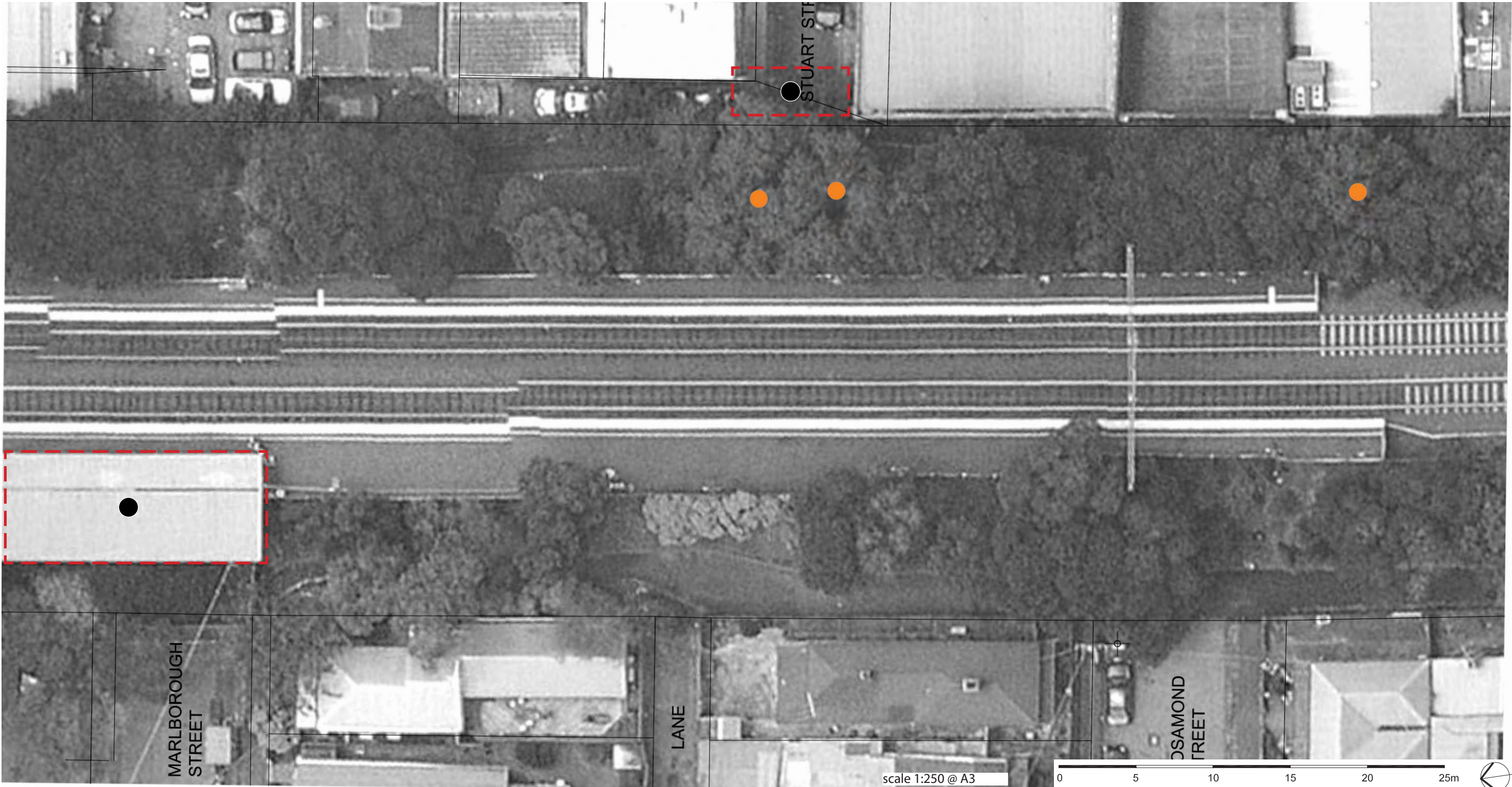


# 4.3

precinct three:  
balaclava walk east and west

- High tree retention value
- ▭ Areas where change will potentially occur. This will be dependant on DoTs final design for the station.
- Removal of fencing between Stuart Street and Balaclava Walk East.

before

















# 4.3

after

## Precinct design materials legend

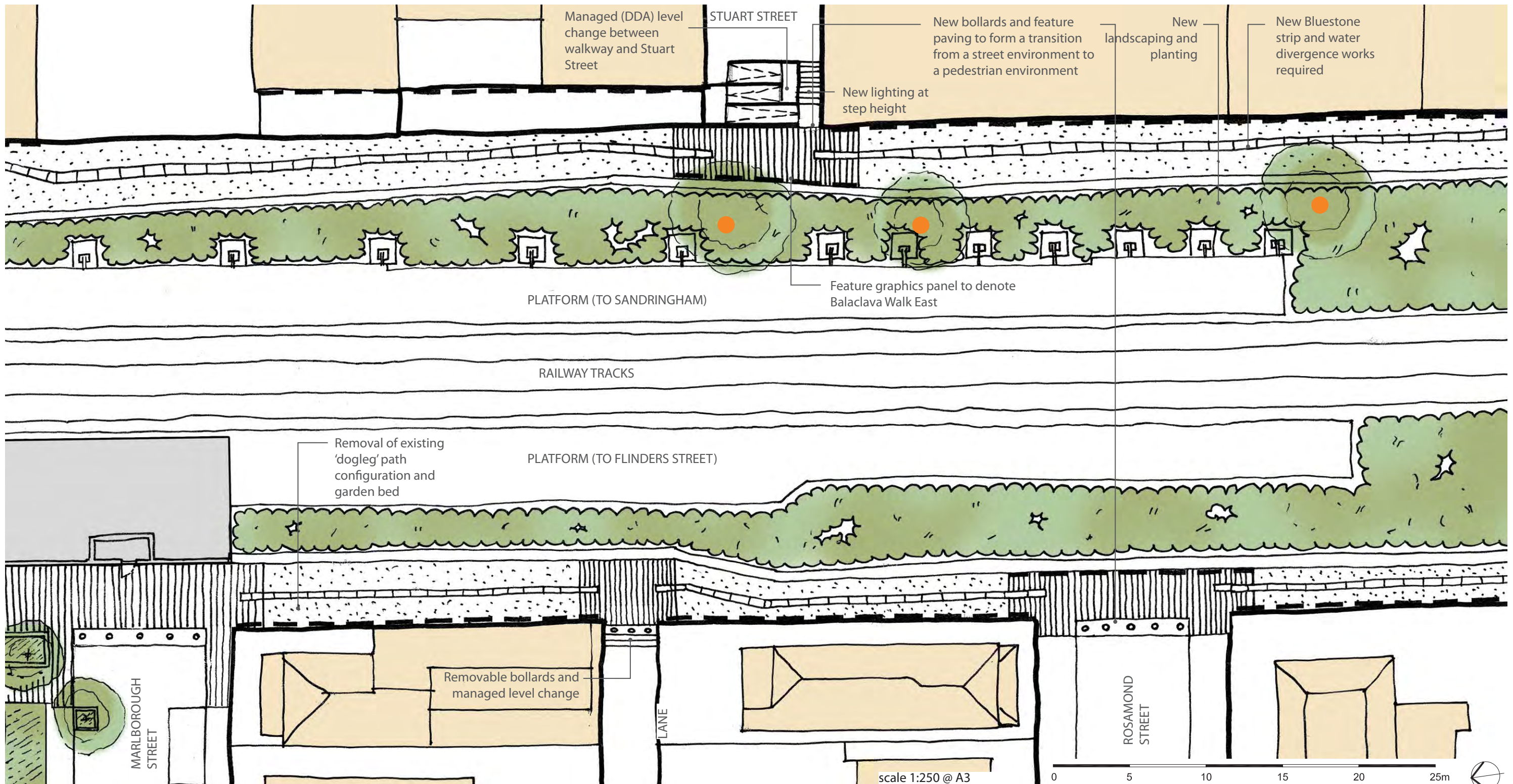
	Existing built form		Bluestone paving
	Proposed Balaclava Station roof outline		Asphalt with Bluestone paving strip
	Garden bed/small-scale planting		Ramp (materials to be determined)
	Large-scale planting		Graphic design element applied to wall/fenceline
	Tree		Bicycle hoops
	High tree retention value		Bollards

## Design response

- > Widen Balaclava Walk East and West where possible
- > Resurface the walkways with high quality surface treatments
- > Replace the makeshift laneway lighting with a more permanent and appropriate lighting design
- > Improve the railway embankment by masking the railway platform and introducing drought tolerant, native plantings
- > Widen intersections to improve sightlines and reduce conflicts and carefully manage level changes between Balaclava Walk East and West and surrounding streets/laneways
- > Improve Rosamond Street's easterly vista by masking the

station platforms and improving the railway embankment landscaping

- > Replace vehicle bollards at the eastern end of Rosamond Street and design a smooth transition from a street environment to a pedestrian environment
- > Encourage light industry and commercial uses on William Street to open up onto Balaclava Walk East
- > Open up the western end of Stuart Street to Balaclava Walk East and manage the level change with an accessible ramp and stairs.





precinct three:  
balaclava walk east and west



Sketch of the future Balaclava Walk East looking north to the proposed station entrance and Carlisle Street beyond





Sketch of the future Balaclava Walk West, looking south towards Nightingale Street



precinct four:  
nightingale street interface

Existing Conditions

- > The railway bridge is quite impressive with its historic Bluestone structure, but needs to be rejuvenated
- > Much of the railway bridge structure is buried beneath poorly maintained garden beds
- > The entrance to the walkway (on both sides of the street) is narrow and does not provide a legible gateway to Balaclava Walk
- > A substation (east of the railway bridge) is unattractive and does little to activate the street frontage
- > Nightingale Street is cluttered by traffic islands and road markings; creating a confusing environment for pedestrians and cyclists.



The view of Nightingale Street from Balaclava Walk West



A neglected garden bed built up in front of the northern Bluestone railway bridge abutment



The railway bridge over Nightingale Street - looking south-west



A close up of the railway bridge structure



Existing car parking arrangements under the railway bridge



An unsightly electrical substation on the south side of Nightingale Street, to the east of the railway bridge



The intersection of Nightingale Street and William Street - looking north





Photomontage of the current Nightingale Street interface - looking west



# 4.4

precinct four:  
nightingale street interface

before

- Very high tree retention value
- High tree retention value
- ▤ Areas where change will potentially occur. This will be dependant on DoTs final design for the station.
- Removal of garden beds around Bluestone railway bridge abutments
- Removal of car parking on either side of the railway bridge
- Removal of traffic islands and road markings on Nightingale Street.



scale 1:250 @ A3

0 5 10 15 20 25m





# after

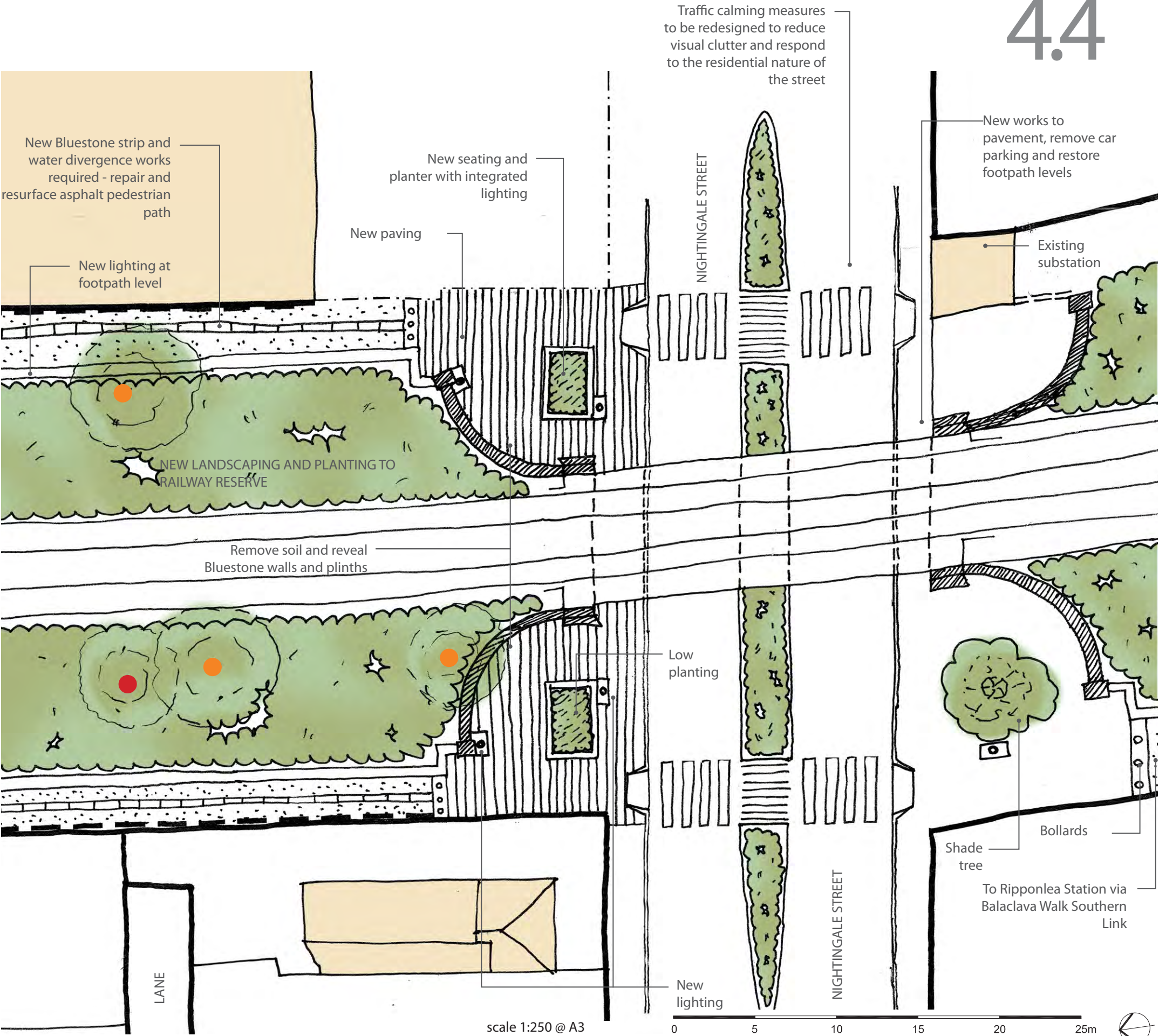
## Design response

- > Remove garden beds at the foot of the railway bridge and uncover the railway bridge structure to widen the entry to the walkway; potentially creating small plaza spaces on either side of the bridge
- > Incorporate gateway elements to communicate the entry to Balaclava Walk East and West
- > Install signage and wayfinding devices into the space to direct people to the station and promote local destinations
- > Provide masking for the electrical substation
- > Provide a common street treatment to the area, to help unify the space and announce the gateway to Balaclava Walk East and West
- > Incorporate traffic calming measures to Nightingale Street and provide a generous and clear pedestrian crossing
- > Widen footpaths on Nightingale Street so that they meet the piers of the railway bridge.

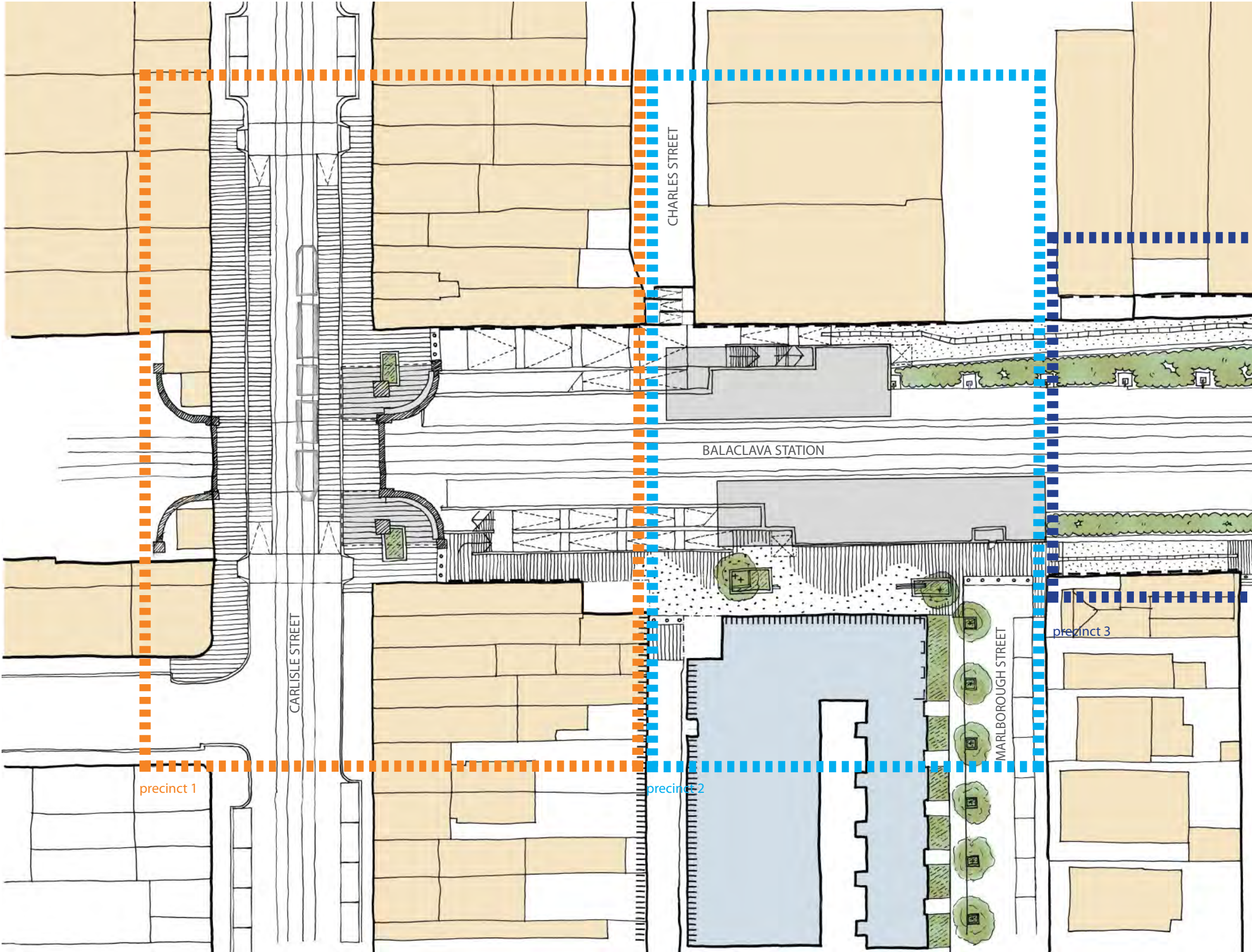
## Precinct design materials legend

- Existing built form
- Garden bed/small-scale planting
- Large-scale planting
- Tree
- Bluestone paving
- Asphalt with Bluestone paving strip
- Graphic design element applied to wall/fenceline
- Bicycle hoops
- Bollards
- New high quality pole lighting

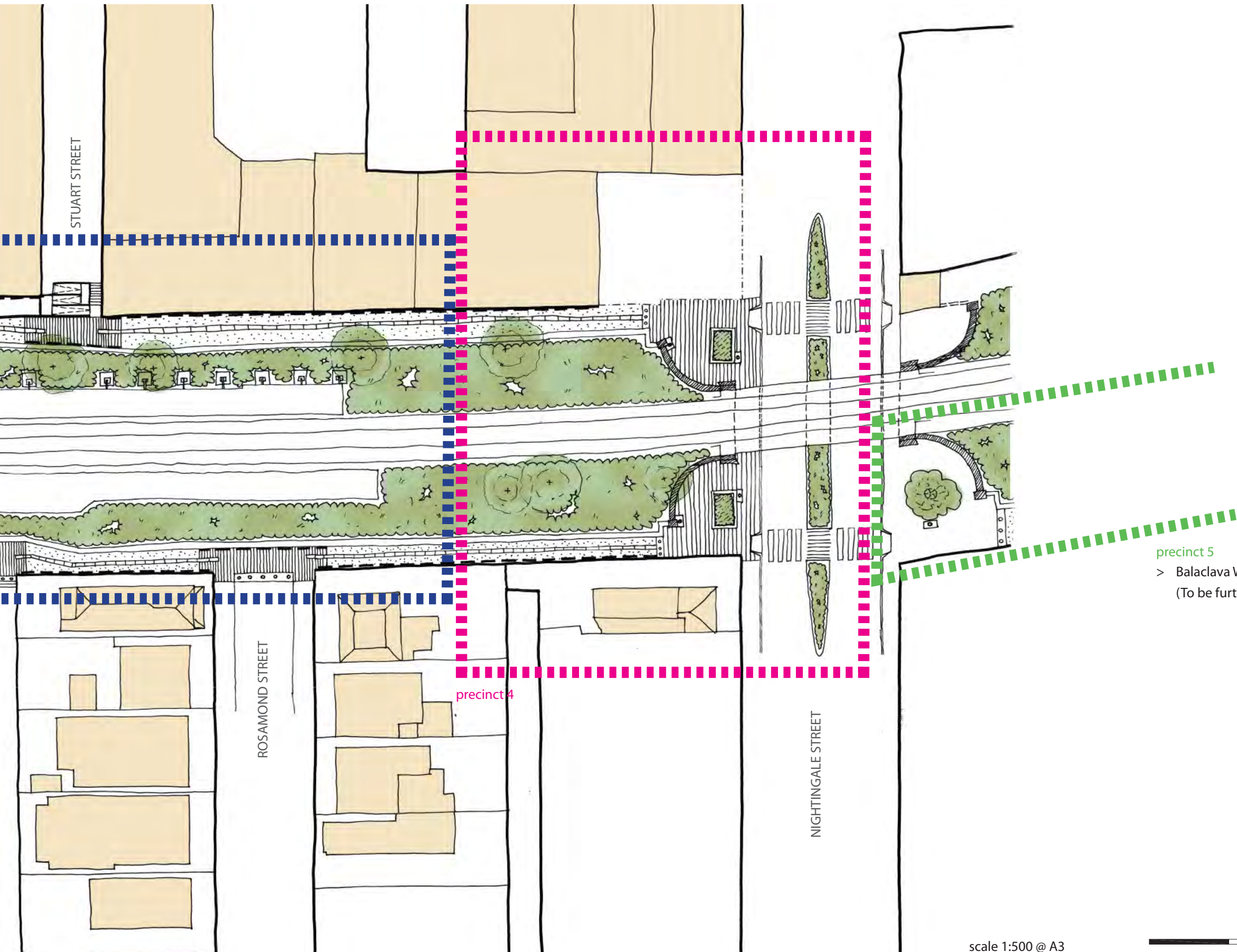
- High tree retention value
- Very high tree retention value (Cork Oak)











precinct 5  
> Balaclava Walk Southern Link to Ripponlea Station  
(To be further detailed in Part 2 of the Masterplan)

scale 1:500 @ A3





materials and accessibility

38 Use high quality ground treatments throughout the public realm

- > Ensure ground treatment materials are durable, non-slip and require little maintenance
- > Ensure ground treatments can accommodate service and emergency vehicle loads
  - The southern lane running parallel to Carlisle Street, the interface between the entry to Balaclava Station and the housing development
- > Use ground treatments to delineate change of function:
  - pedestrian/bicycle zone
  - vehicle zone
  - shared pedestrian/bicycle/vehicle zone
- > Use changes in ground treatment to indicate changes in ownership:
  - public ownership
  - semiprivate ownership
  - private ownership.

Use high quality street furniture and fittings throughout the public realm

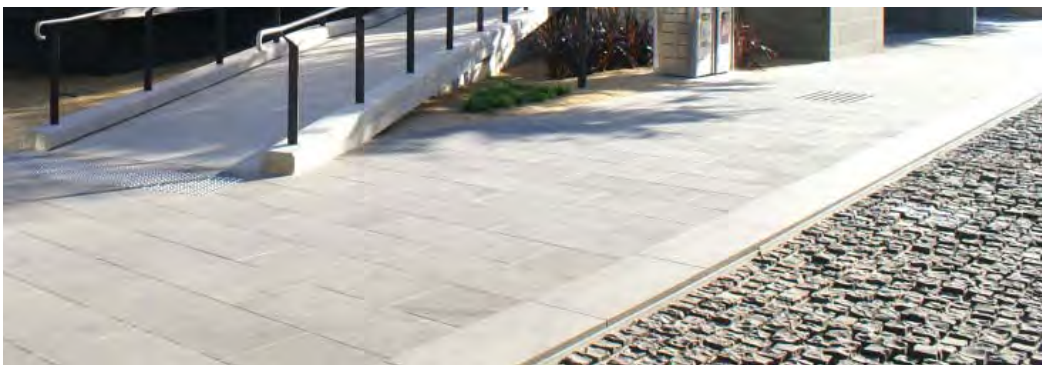
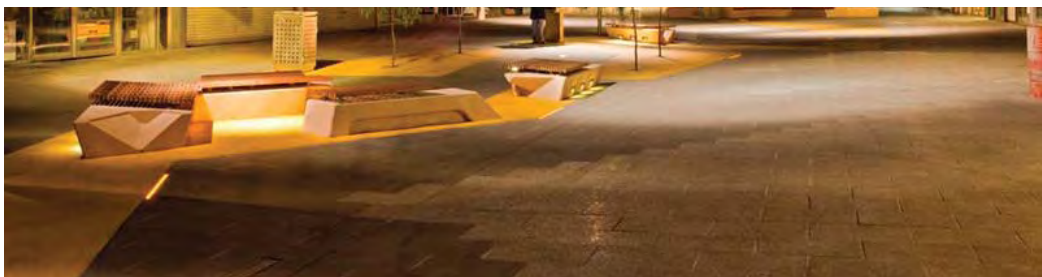
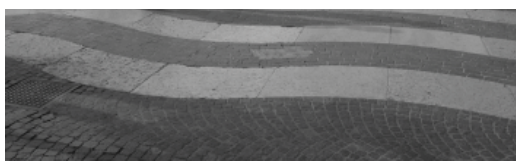
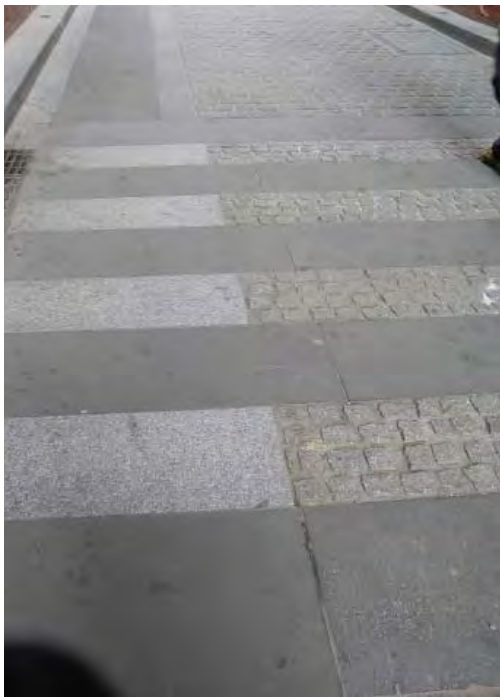
- > Provide a consistent palette of street furniture and fittings throughout the Masterplan precincts
- > Ensure that street furniture elements are durable and require little regular maintenance
- > Integrate necessary services infrastructure into built form or landscape elements to reduce visual clutter
- > Remove superfluous and obsolete street furniture.

Provide access for all

- > Ensure that the public realm addresses the needs of everyone; especially the elderly, those with disabilities, and people with prams
- > Ensure that spaces throughout the study area are Disability Discrimination Act (DDA) compliant
- > Provide a ramp or reliable lift where changes in level exist.

Develop detailed urban/landscape designs for each precinct

- > Ensure that landscape architects and urban designers are engaged to develop detailed designs for each precinct; paying particular attention to accessibility requirements and the use of materials throughout the area.



Above: various changes in paving material or pattern

Bluestone should feature throughout all the precincts. When necessary, it can be combined with less expensive materials, such as asphalt, and still provide a high quality urban finish. Precinct One and Two will be prioritised over Precinct Three and Four for allocation of Bluestone paving and surface treatments.



Above: negotiated changes in level

Changes of level offer opportunities for informal recreation and relaxation. The proposed level changes on Charles Street and Stuart Street will be designed to ensure maximum accessibility.





Above top: an example of street art panelling in the Marlborough Street car park  
 Above bottom: graphic panelling can be installed to act as a buffer between walkways and private property

Existing street art panelling can be relocated to key areas of Balaclava Walk East and West; helping with wayfinding and providing a buffer to adjacent residential uses. Graphic panelling can act in the same way and can be freestanding or attached to buildings.



Above: seating installed along walkways and around existing landscape elements

Provide places for rest along Balaclava Walk East and West, and in popular gathering places, such as the Carlisle Street transport node.



Above: high quality street furniture elements

The installation of street furniture throughout the Masterplan precincts should be consistent.



Above: removable bollards

The bollards used throughout the precincts should be durable and closely match the street furniture palette. They should also be easily removable when necessary.



planting and water

40

March 2012

Balaclava Walk Masterplan: Balaclava Station and Environs

- Follow the advice of the Balaclava Walk Arboricultural Impact Assessment Report (December 2010) where possible
- > Remove over-mature trees, such as some of the Wattles, in the next few years
  - > Retain Peppercorn trees where possible, as they form a strong and significant landscape element throughout the area
  - > Incorporate high retention value trees into precinct landscape designs
  - > Incorporate the very high retention value tree (Cork Oak) into its precinct landscape design
  - > Protect high retention value trees where possible
  - > Protect very high retention value trees
  - > Incorporate the existing Bursaria trees into the precinct landscape designs where possible
  - > Implement protection measures to minimise inadvertent damage to tree roots and canopies during construction activities.

- Plant indigenous trees and shrubs on the eastern railway embankment
- > Plant the eastern embankment with hardy, indigenous evergreen trees that complement the existing high retention value trees
  - > Ensure that Balaclava Walk East is not overshadowed by the selected plantings
  - > Select durable plants, which need little maintenance and hinder the growth of weeds
  - > Select plants that reduce erosion of the eastern railway embankment.

- Plant deciduous or exotic species on the western railway embankment
- > Select plant species that provide winter light access along Balaclava Walk West, such as deciduous or exotic species
  - > Ensure that the overshadowing of Balaclava Walk West is kept to a minimum, especially during cooler months
  - > Select durable plants, which need little maintenance and hinder the growth of weeds
  - > Select plants that reduce erosion of the western railway embankment.

- Develop detailed urban/landscape designs for each precinct
- > Ensure that landscape architects and urban designers are engaged to develop detailed designs for each precinct; paying particular attention to planting and soft landscaping.
  - > Encourage alternative greening techniques to offset the loss of vegetation, such as green walls and roofs

- Use best practice stormwater management
- > Harvest, store, treat and use stormwater on site
  - > Treat and re-use stormwater for landscaping



Above: potential low level / groundcover plantings (Two species of Grevillea)

Plant hardy, drought-tolerant, groundcover species on the railway embankments and in feature planter boxes throughout the Masterplan precincts.



Above: tree pit a potential method of re-using and treating stormwater on site

Treat and re-use stormwater for landscaping



Above: potential low level / groundcover plantings (Dianella revoluta)

Plant hardy, drought-tolerant, groundcover species on the railway embankments and in feature planter boxes throughout the Masterplan precincts.



Above: two examples of re-using and treating stormwater on site

Treat and re-use stormwater for landscaping





Above: potential indigenous evergreen tree plantings (Coast Banksia, *Banksia integrifolia*)

Plant hardy, drought-tolerant, indigenous species on the eastern railway embankment, and ensure they complement the existing high retention value trees throughout the precincts.



Above: potential indigenous evergreen tree plantings (Sheoak, *Allocasuarina* sp.)

Plant hardy, drought-tolerant, indigenous species on the eastern railway embankment, and ensure they complement the existing high retention value trees throughout the precincts.

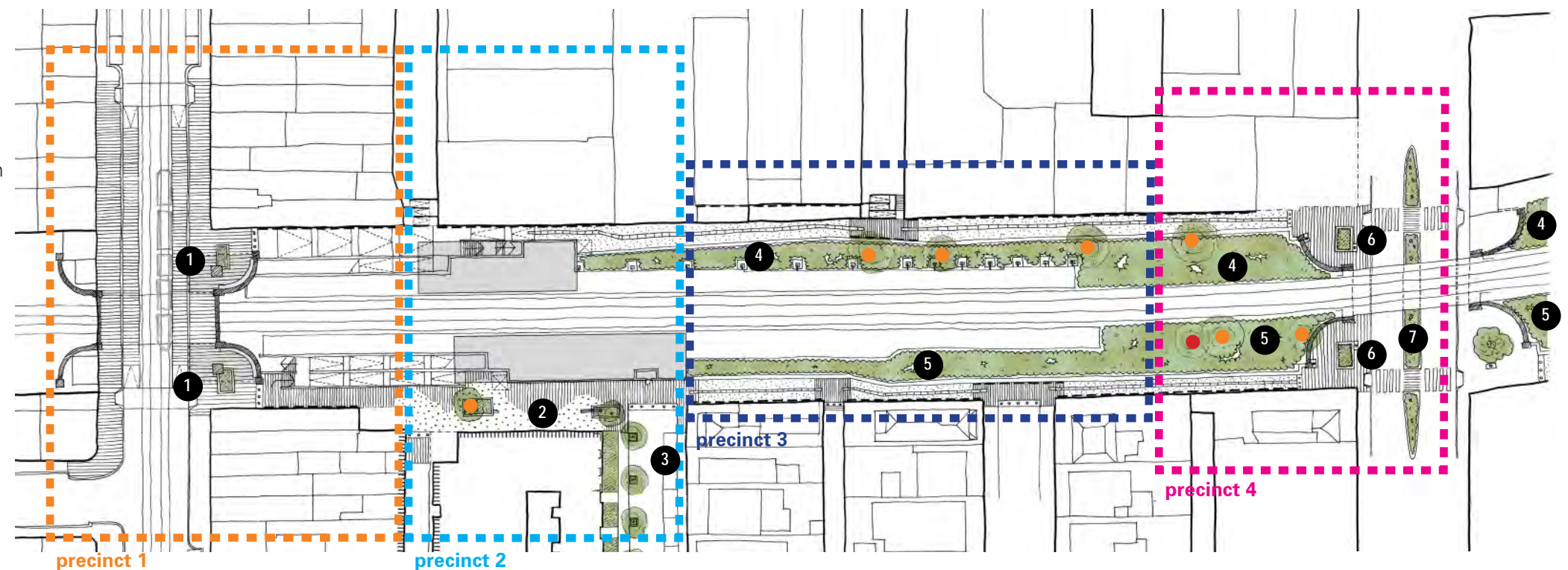


Above: potential deciduous tree plantings (Honey Locust, *Gleditsia tricanthos* 'Limegold')

Plant tree species on the western railway embankment that provide winter light access along Balaclava Walk West. These may include deciduous or exotic species, but must complement the existing high retention value trees within the area.

#### Proposed planting in the Masterplan study area

- Protect the existing very high retention value tree:  
- Cork Oak (*Quercus suber*)
- Protect the existing high retention value trees:  
- A mixture of Narrow-leaved Peppermint (*Eucalyptus nicholii*), Peppercorn Tree (*Schinus areira*), River Red Gum (*Eucalyptus camaldulensis*) and Yellow Gum (*Eucalyptus leucoxylon*)
- 1 Planter boxes integrated into the tram interchange design
- 2 Planter boxes and existing trees integrated into Balaclava Station and the housing development interface
- 3 New street trees along Marlborough Street
- 4 Indigenous, evergreen trees and groundcover
- 5 Deciduous trees and indigenous groundcover
- 6 Planter boxes integrated into the Nightingale Street public realm improvements
- 7 A median strip with low level indigenous plantings in the middle of Nightingale Street



scale 1:1000 @ A3

0 10 20 30 40 50m



lighting and signage

42

- Provide well-considered and creative lighting throughout the area
- > Employ a well-considered and creative lighting design to encourage night time activity, create a safe environment and emphasise key buildings
  - > Provide a mix of lighting types throughout the Masterplan precincts; including overhead street lamps, wall sconces, bollards, and lighting integrated into stairs, ramps and retaining walls
  - > Ensure that vehicle/pedestrian/bicycle conflict zones are well lit
  - > Use the light generated by surrounding businesses to add another visual dimension to the public realm
  - > Avoid unnecessary light pollution by employing an appropriately scaled lighting design throughout the Masterplan precincts.

Provide signage to enhance legibility throughout the area

- > Ensure that signage is consistent throughout the Masterplan precincts
- > Concentrate signage at popular pedestrian nodes and landmarks
- > Use materials, such as paving and tiles, to create implicit pedestrian wayfinding devices throughout the Masterplan precincts
- > Incorporate graphics into ground and wall treatments to aid in wayfinding
- > Remove redundant/misleading signage from Balaclava Walk and the surrounding area
- > Ensure that advertising signage makes a positive and innovative contribution to the urban environment.



Above: an example of lamps integrated into stair elements



Above: City of Port Phillip standard urban light

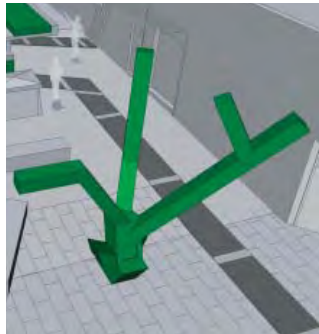


Above: an example of walkway lighting

Integrate lighting into bollards and on the edges of walkways.



Above: examples of visually engaging lighting elements in the public realm

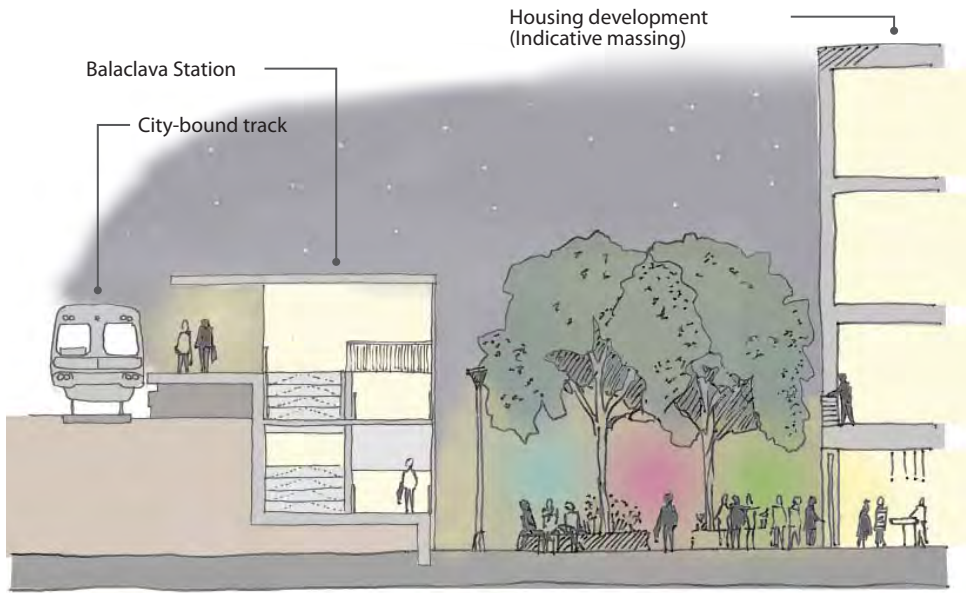


Above: examples of engaging public realm wayfinding devices

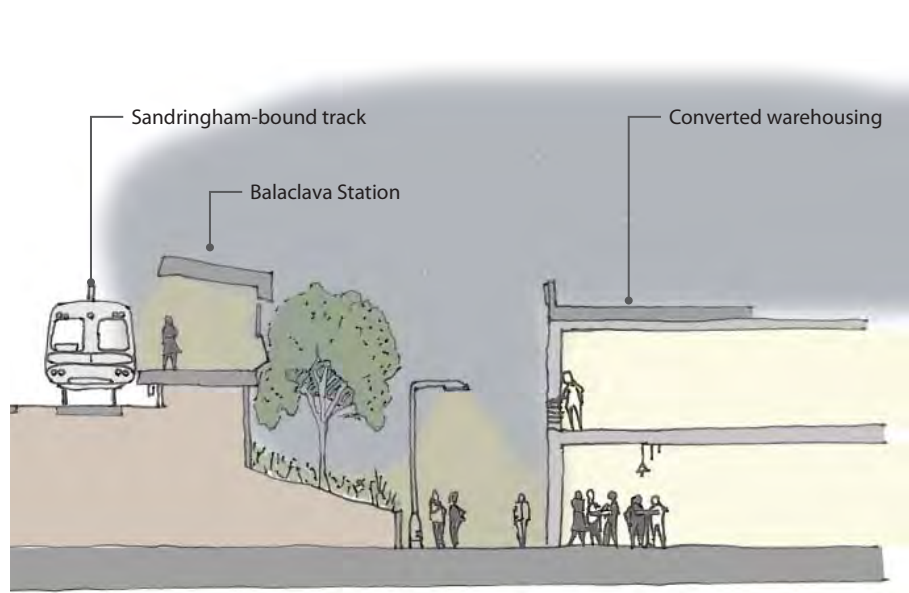
Provide signage throughout all the Masterplan precincts to enhance wayfinding and provide relevant information about public transport and local attractions.



Provide a considered and playful lighting scheme between the housing development and new development to the immediate south of Carlisle Street  
Indicative section view of the lane running parallel to the south of Carlisle Street, looking west to Woodstock Street



Provide a considered and playful lighting scheme between the housing development and the newly upgraded Balaclava Station  
Indicative section view of Balaclava Walk West, looking south to Marlborough Street



Provide a considered and vibrant lighting scheme along Balaclava Walk East and West, mainly through the use of City of Port Phillip standard urban lights  
Indicative section view of Balaclava Walk East, looking north to Carlisle Street



# safety and security

Ensure natural surveillance of Balaclava Walk and surrounds

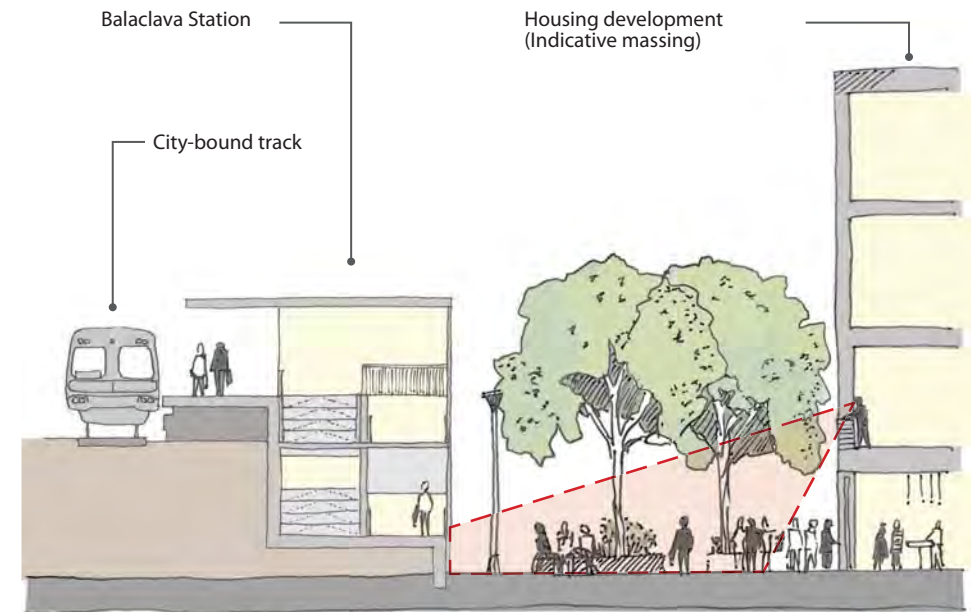
- > Ensure building frontages throughout the precinct are activated and front onto the public realm
  - Housing development, businesses backing onto the lane running parrallel between Carlisle and Marlborough Streets, warehousing backing onto Balaclava Walk East
- > Put 'eyes on the street' where possible
  - Between Balaclava Station and the housing development, along Balaclava Walk East and West
- > Provide a mix of uses at street level to add vitality to the area, both day and night
  - Housing development, businesses backing onto the lane running parrallel between Carlisle and Marlborough Streets, warehousing along Balaclava Walk East
- > Encourage building uses to overlap, in order to achieve day/night activation
- > Open up vulnerable, dead-end streets and laneways
  - Along Balaclava Walk East and West
- > Provide a mix of lighting in the public realm (overhead lamps, bollards, wall sconces and from adjacent uses)
- > Use appropriate plantings in precinct landscape designs (avoid planting that is too high or dense).

Minimise modal conflict on Balaclava Walk

- > Ensure that walkways are wide enough to accommodate the safe passage of pedestrians
  - Balaclava Walk East and West, between Carlisle Street and Balaclava Station
- > Ensure that walkways provide clear sightlines for pedestrians
  - Balaclava Walk East and West.

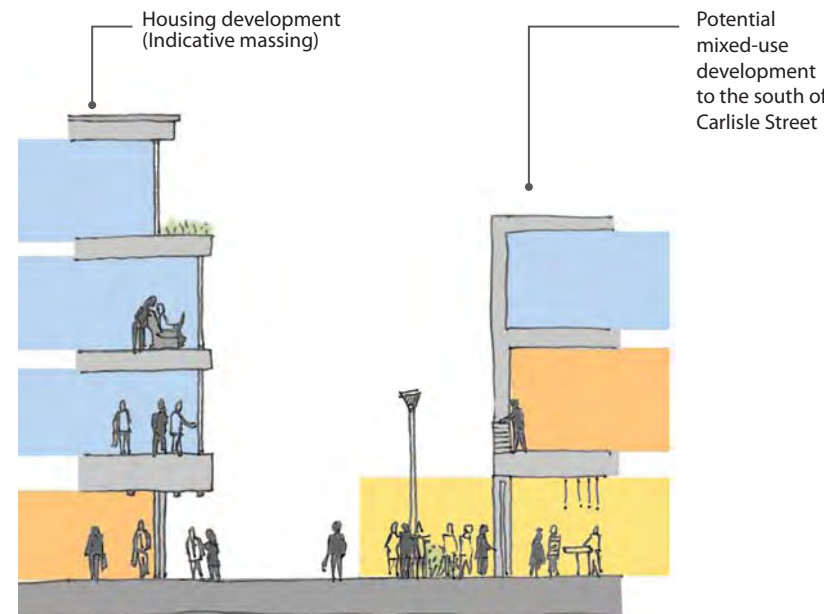
Design in territoriality throughout the Balaclava Station environs

- > Provide changes in materials (pavement, landscaping, canopies, etc) to mark where public space ends and private/semiprivate transition zones begin
  - Immediate area around the housing development
- > Conduct regular maintenance of built form and landscaping, in order to show that there is an alert and active presence who regularly visits the space.



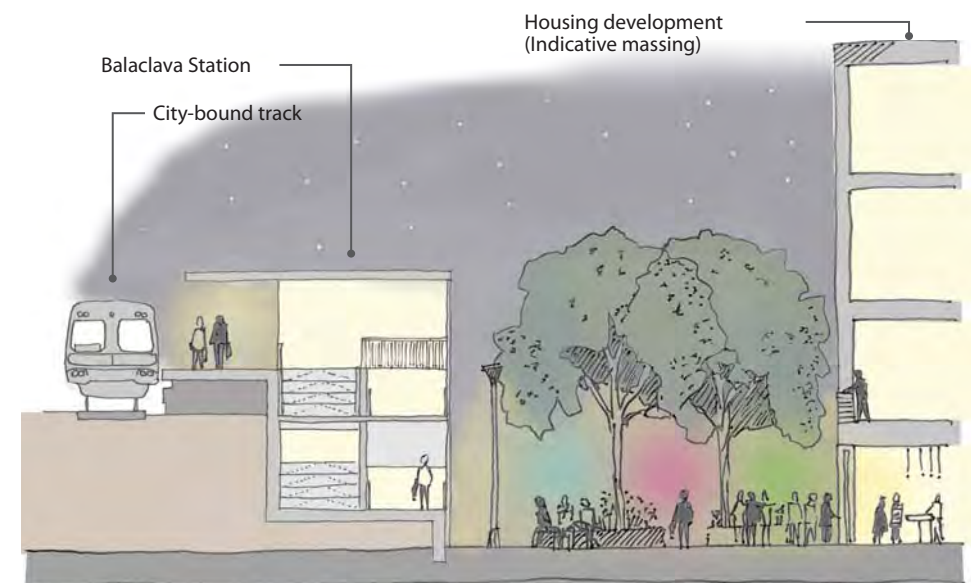
Ensure the housing development and Balaclava Station upgrade provide opportunities for passive surveillance of the public realm

Indicative section view of Balaclava Walk West, looking south to Marlborough Street



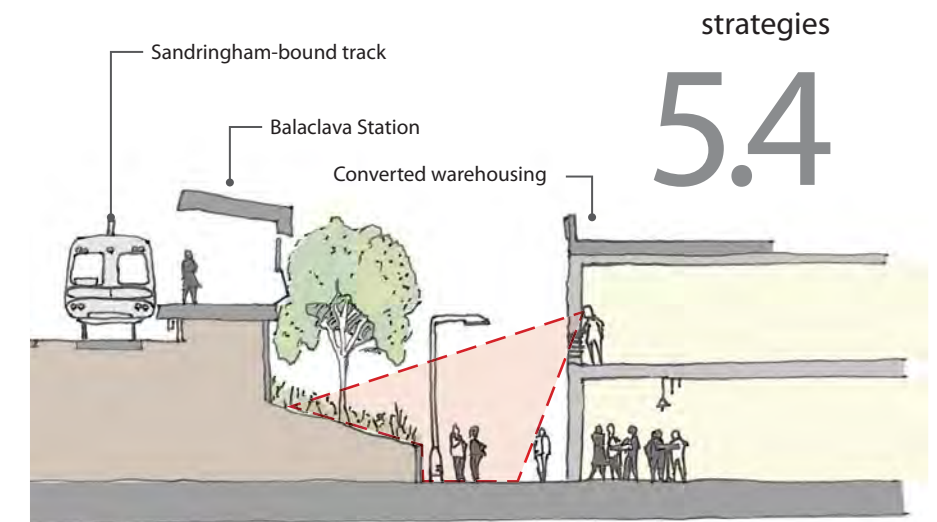
Encourage activated frontages and a mix of overlapping building uses throughout the Masterplan study area to ensure day/night activation and a safe pedestrian environment

Indicative section view of the lane running parallel to the south of Carlisle Street, looking west to Woodstock Street



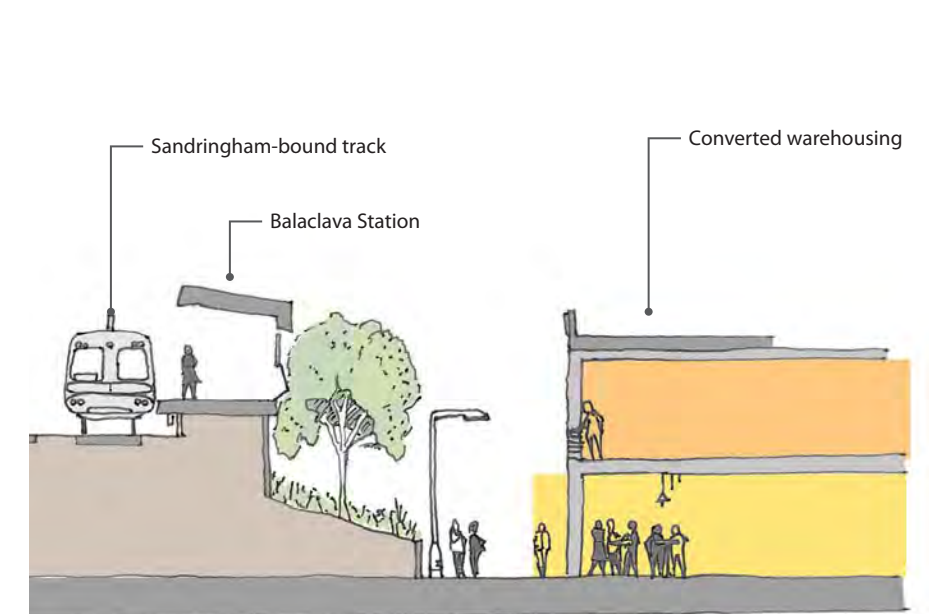
Provide a considered and vibrant lighting scheme between the housing development and the newly upgraded Balaclava Station

Indicative section view of Balaclava Walk West, looking south to Marlborough Street



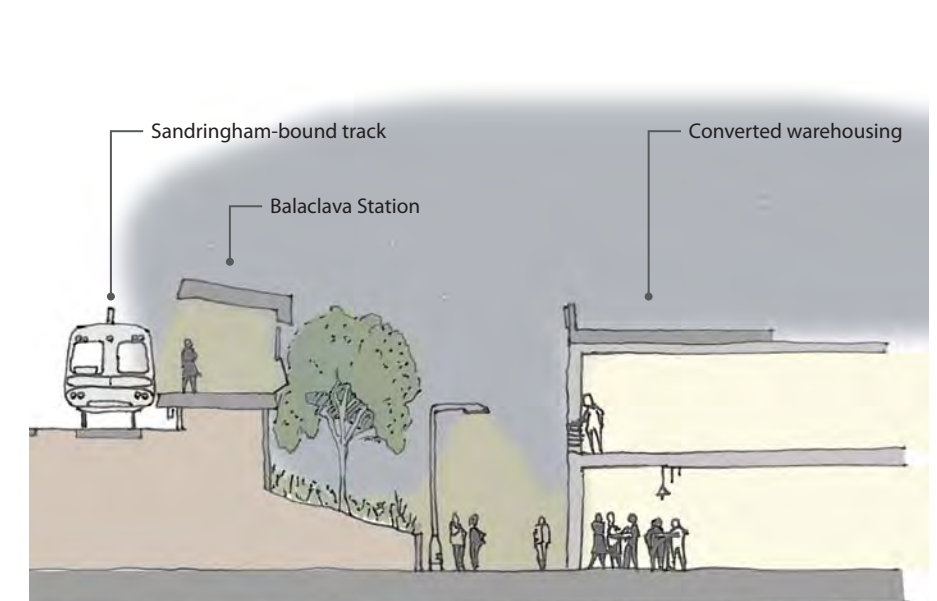
Ensure that new development abutting Balaclava Walk East and the Balaclava Station upgrade provides opportunities for passive surveillance of the public realm

Indicative section view of Balaclava Walk East, looking north to Carlisle Street



Encourage activated frontages and a mix of overlapping building uses along Balaclava Walk East to ensure day/night activation and a safe pedestrian environment

Indicative section view of Balaclava Walk East, looking north to Carlisle Street



Provide a considered and vibrant lighting scheme along Balaclava Walk East and West, and seek to reduce low lit areas

Indicative section view of Balaclava Walk East, looking north to Carlisle Street

strategies  
5.4



integrated art

44

- Integrate public artwork along Balaclava Walk and the surrounding area
- > Encourage public/urban art that expresses the identity and cultural heritage of the community
  - > Develop a cohesive artistic treatment that helps to unify the various Masterplan precincts
  - > Use playful and engaging sculptural pieces at key points in the public realm or as gateway elements to Balaclava Walk East and West. Sculptures could be installed at the Carlisle Street interface, as well in the area between the western Balaclava Station entry and the housing.
  - > Investigate the potential for the relocation of The Lady of St Kilda sculpture to a prominent site near Balaclava Station
  - > Liase with DoT and the housing development to formulate an allied approach to integrated art on Balaclava Walk West between Carlisle Street and Marlborough Street
  - > The process detailed above assumes the appointment of an arts curator and/or manager for the Masterplan area (this role could be fulfilled by City of Port Phillip arts officers or suitably qualified external consultants.

Pursue high quality and enduring public art

- > Ensure that public art around Balaclava Station and its surrounds:
  - results from a collaboration between the community of the site (with its idiosyncratic values and beliefs), the private sector and the art/design community (including cross discipline linkages) as appropriate to each Urban Art project.
  - is managed by a process that allows creative ideas to manifest, ranging from purely aesthetic modes through to highly innovative forms that challenge notions of art in the public domain.

For more detail, refer to the City of Port Phillip Urban Art Strategy, June 2002.

Apply the Urban Art Design Evaluation Framework to new public art

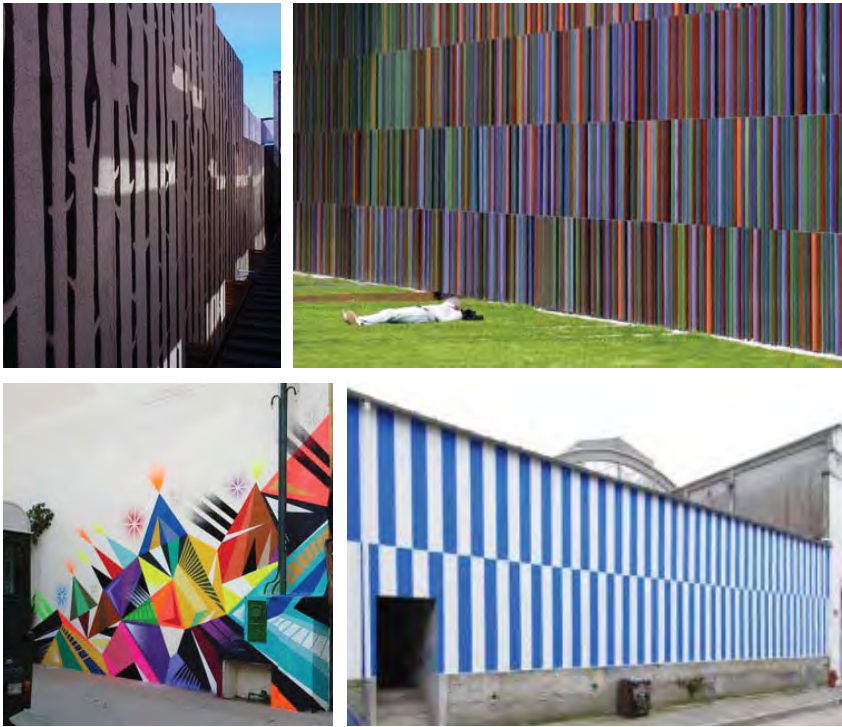
- > Ensure that public/urban art is assessed against Council's six stated design intentions, which are:
  - Identity of Place
  - Community Values
  - Innovation
  - Aesthetic Appeal
  - Functionality
  - Utility.

These six design intentions are detailed further in the City of Port Phillip Urban Art Strategy, June 2002.

Follow Council's preferred sequence of project management for integrated art throughout the Masterplan precincts

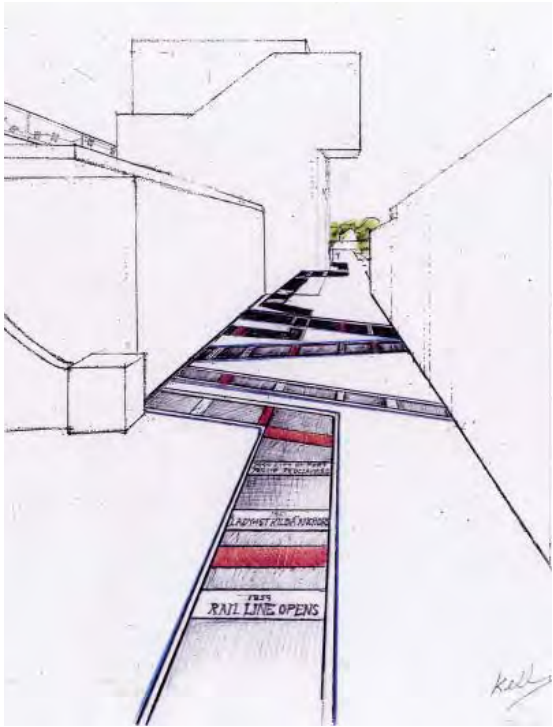
- > Refer to the accompanying diagram (right) for the preferred system of integrated art project management

For more detail on integrated art project management, refer to the Urban Art Information Kit, contained within the City of Port Phillip Urban Art Strategy, June 2002.



Above: examples of geometric abstraction in the public realm

Engage artists to enliven blank building facades with vibrant, large-scale geometric abstraction murals. These pieces may be inspired by, or respond to, existing street art in the surrounding area, which will add another dimension to Balaclava's public art scene. Geometric murals would most likely be placed on blank facades along Balaclava Walk East.



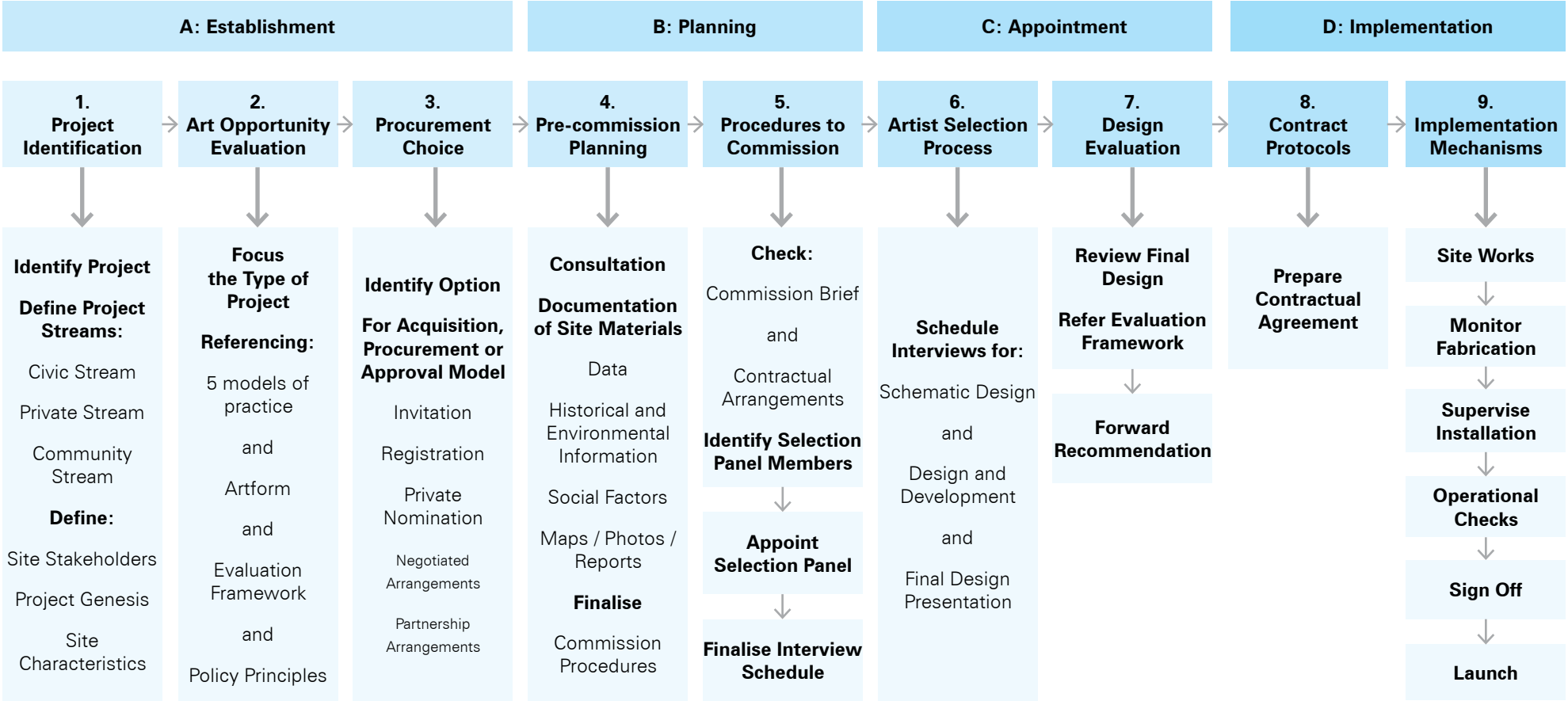
Above: The Journey, by Bill Kelly (initial concept design)

Help to develop artwork that responds to Balaclava's unique character and identity. Site specific work, which is integrated with the surrounding urban/landscape design, should be facilitated by the designated curator of the Masterplan precincts.

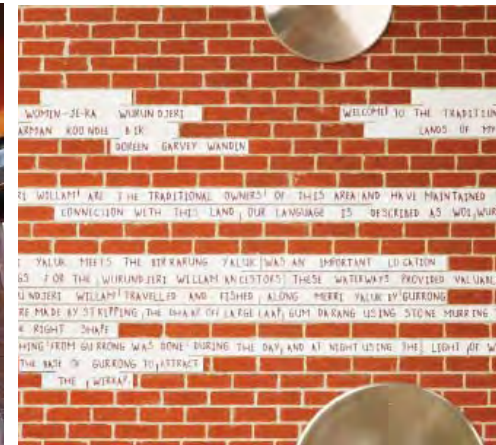


Above: examples of creative paving patterns in the public realm

Provide a high quality paving treatment (Bluestone) in Precinct Two, with integrated graphics and/or changes in paving material. Use paving patterns to develop a narrative around Balaclava Station and to help unify all precincts.







Above: examples of street art around Balaclava Station

Above: the Clifton Hill Rail Bridge, by MGS Architects

Above: an example of creatively designed street furniture

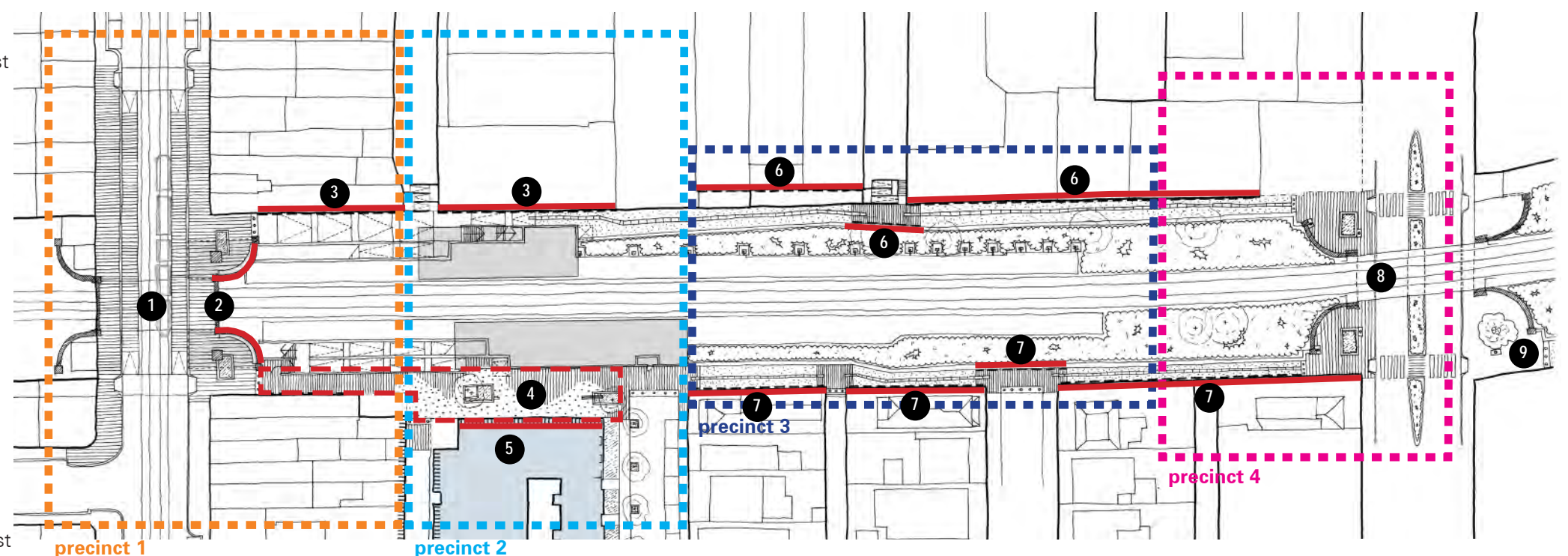
Facilitate the creation of high quality street art throughout Precinct Two and Three to contribute to the 'outdoor gallery' atmosphere and to help create a buffer between Balaclava Walk West and sensitive residential uses.

Take the opportunity to integrate art into existing infrastructure, such as the Carlisle or Nightingale Street railway bridges.

Integrate art into everyday street furniture elements throughout the Masterplan study area.

#### Proposed integrated art locations

- 1 Northern gateway element to Balaclava Walk East and West
- 2 Heritage sensitive wall treatments at Carlisle Street, which celebrate community identity (subtle detailing only)
- 3 Large-scale geometric abstraction or similar alongside eastern platform access ramp
- 4 Striking and playful pavement patterns at the interface between the station entry and the housing
- 5 Use the eastern elevation of the housing as a canvas for artworks to be viewed from Balaclava Station platforms
- 6 Street art-style murals that address Balaclava Walk East (directly painted onto, or fixed onto, buildings)
- 7 Street art-style murals that address Balaclava Walk West and act as a buffer to residential uses (this may include the potential relocation of existing street art panels from the Marlborough Street car park)
- 8 Southern gateway element to Balaclava Walk East and West
- 9 Gateway element to Balaclava Walk Southern Link



scale 1:1000 @ A3

0 10 20 30 40 50m





# 6.1

## project staging

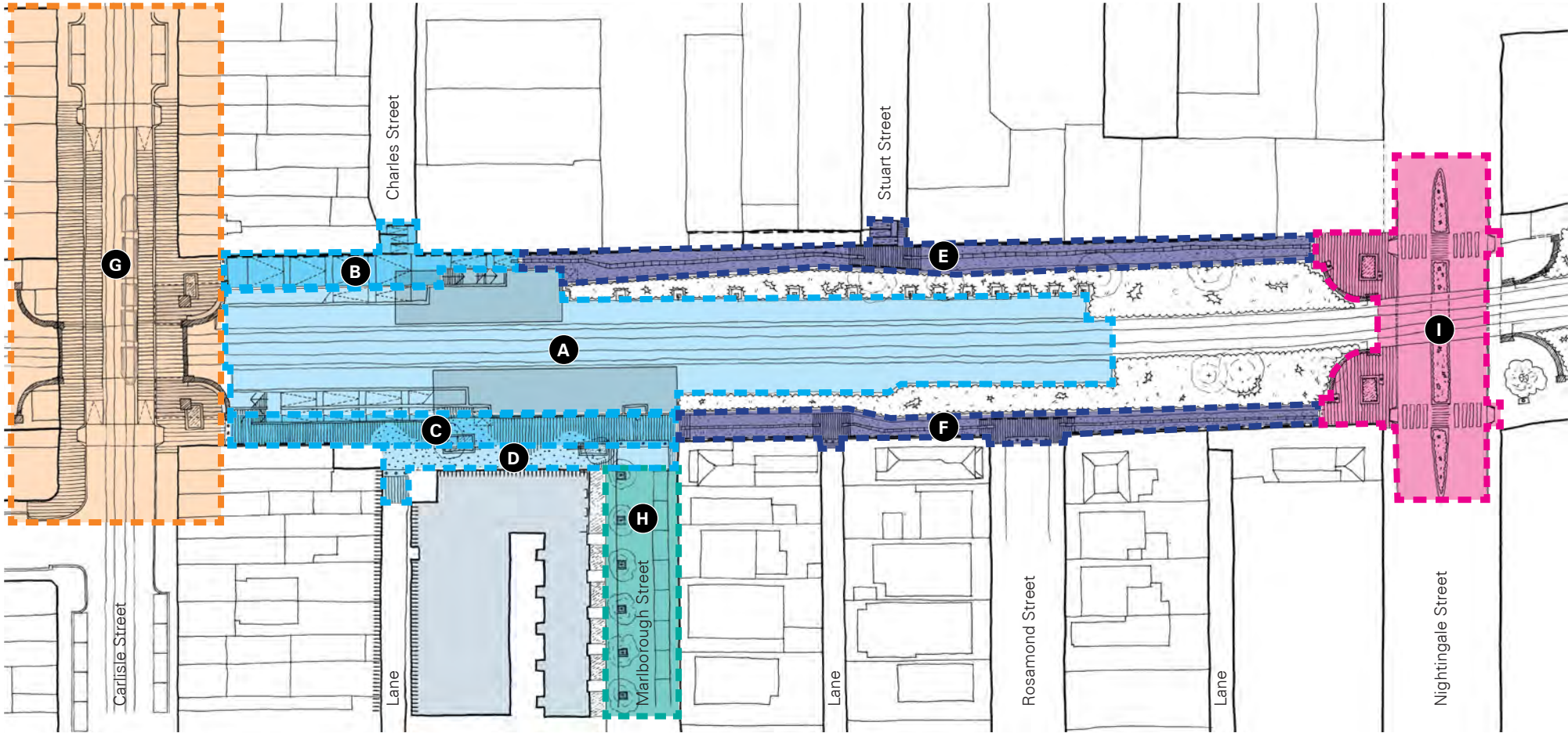
46 Preferred staging for key Masterplan projects  
The below diagram detail the preferred staging method for the key Masterplan projects

BALACLAVA STATION UPGRADE AND BALACLAVA WALK WORKS

- A** Balaclava Station upgrade
- B** Eastern station ramp works
- C** Western station interface
- D** Eastern housing interface
- E** Balaclava Walk East
- F** Balaclava Walk West

FUTURE PUBLIC REALM PROJECTS

- G** Carlisle Street works
- H** Marlborough Street works
- I** Nightingale Street works



scale 1:1000 @ A3