

04 Sub-precincts

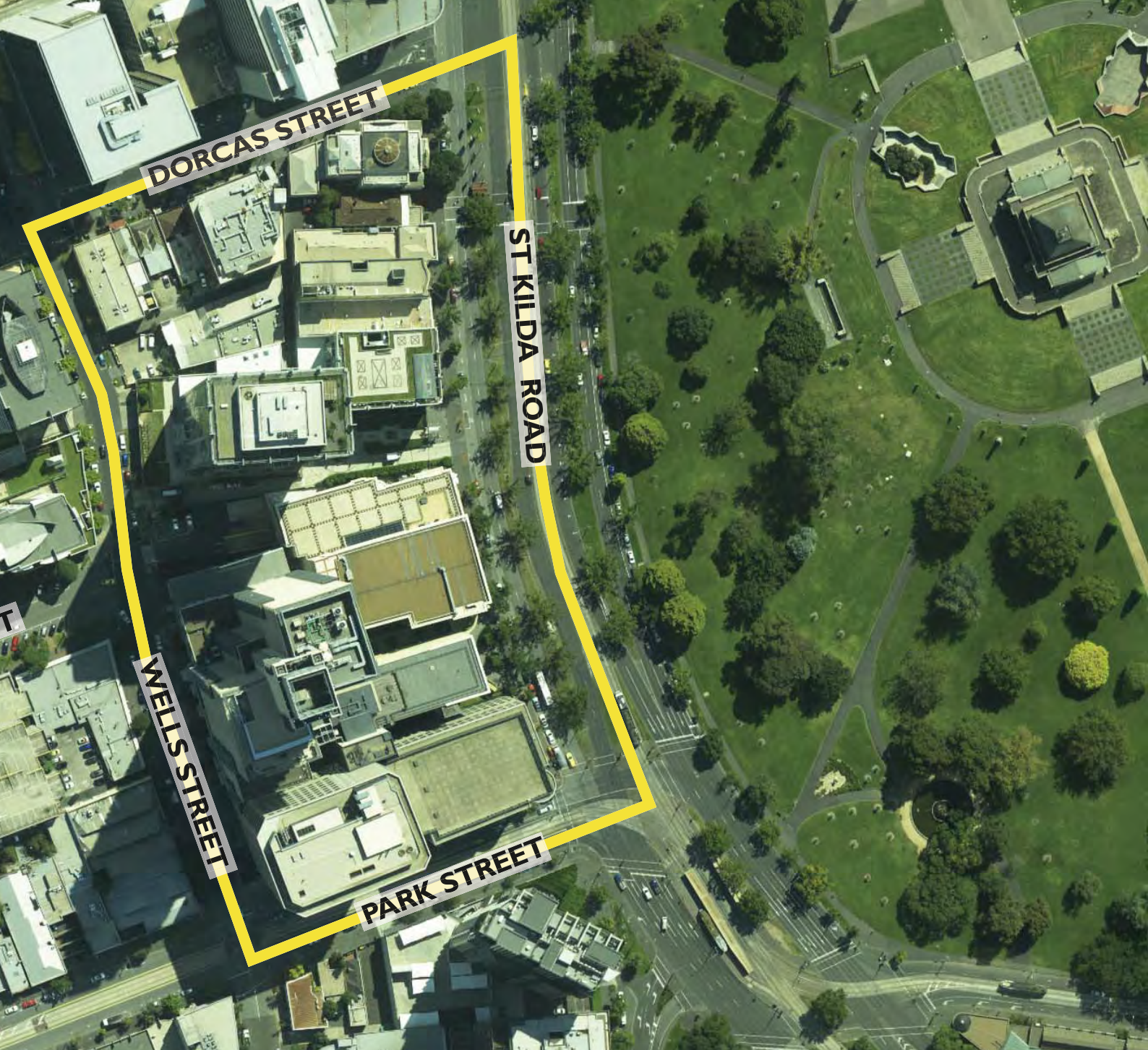
St Kilda Road North Precinct comprises six distinct Sub-precincts. In some Sub-precincts, there is a readily identifiable built form character that will be maintained and strengthened. In other Sub-precincts, an evolution and change of the built form character will occur to create high quality, high amenity and sustainable neighbourhoods.

Sub-precincts

- 1** **EDGE OF SHRINE MEMORIAL GARDENS** forms the western backdrop to the Shrine and the edge to the Memorial Gardens and the Domain Parklands. The scale, form and detailed design elements of buildings within this sub-precinct are critical in respecting the sensitivity of the Shrine setting and will require specific built form controls.
- 2** **NORTHWEST CORNER** has a mixed character and role, with a varying scale and quality of existing buildings and, in many parts, a poor quality of street scape. This Sub-precinct presents considerable opportunity for development and change as a higher density residential or mixed use enclave, based around lively, pedestrian focussed streets.
- 3** **ALBERT ROAD SOUTH** forms the edge of Albert Park and offers a transitional scale to the adjoining heritage residential neighbourhoods and parklands
- 4** **ALBERT ROAD NORTH & BOWEN CRESCENT** is distinct as a part of the study area that expresses a unique nineteenth century formal street layout. Here, higher scale buildings clustered around the Domain will serve as focal point within the study area and a point of transition along the St Kilda Road axis.
- 5** **ST KILDA ROAD SOUTH OF KINGS WAY** forms the southern section of the St Kilda Road boulevard between Kings Way and the Junction. Here, the street scape is strongly defined by the scale and form of regularly spaced buildings, the consistent boulevard planting and the wide, straight road reserve with multiple lanes.
- 6** **QUEENS ROAD** is distinct as a lower rise, predominantly residential area that forms the edge to Albert Park. There is a consistency of building scale and siting that creates a cohesive street



MAP 9: SUB-PRECINCTS MAP



4.1 SUB-PRECINCT 1

Edge of Shrine Memorial Gardens



VISION: EDGE OF SHRINE MEMORIAL GARDENS

Sub-precinct I will be characterised by:

- buildings of a scale, form and detailed design that form a respectful background to the Shrine of Remembrance and Memorial Gardens*
- buildings which are designed to respect the sensitivity of the Shrine setting within its immediate vicinity and from more distant elevated points of the Shrine site*
- new development that reinforces the established pattern of consistent low scale deep podium form at street edge with high towers behind*
- incremental redevelopment that supports a growing residential population containing complementary commercial and community activities*
- consistent street tree planting that contributes to the high amenity of the street scape*
- an active, high quality pedestrian environment at street level.*

DESCRIPTION

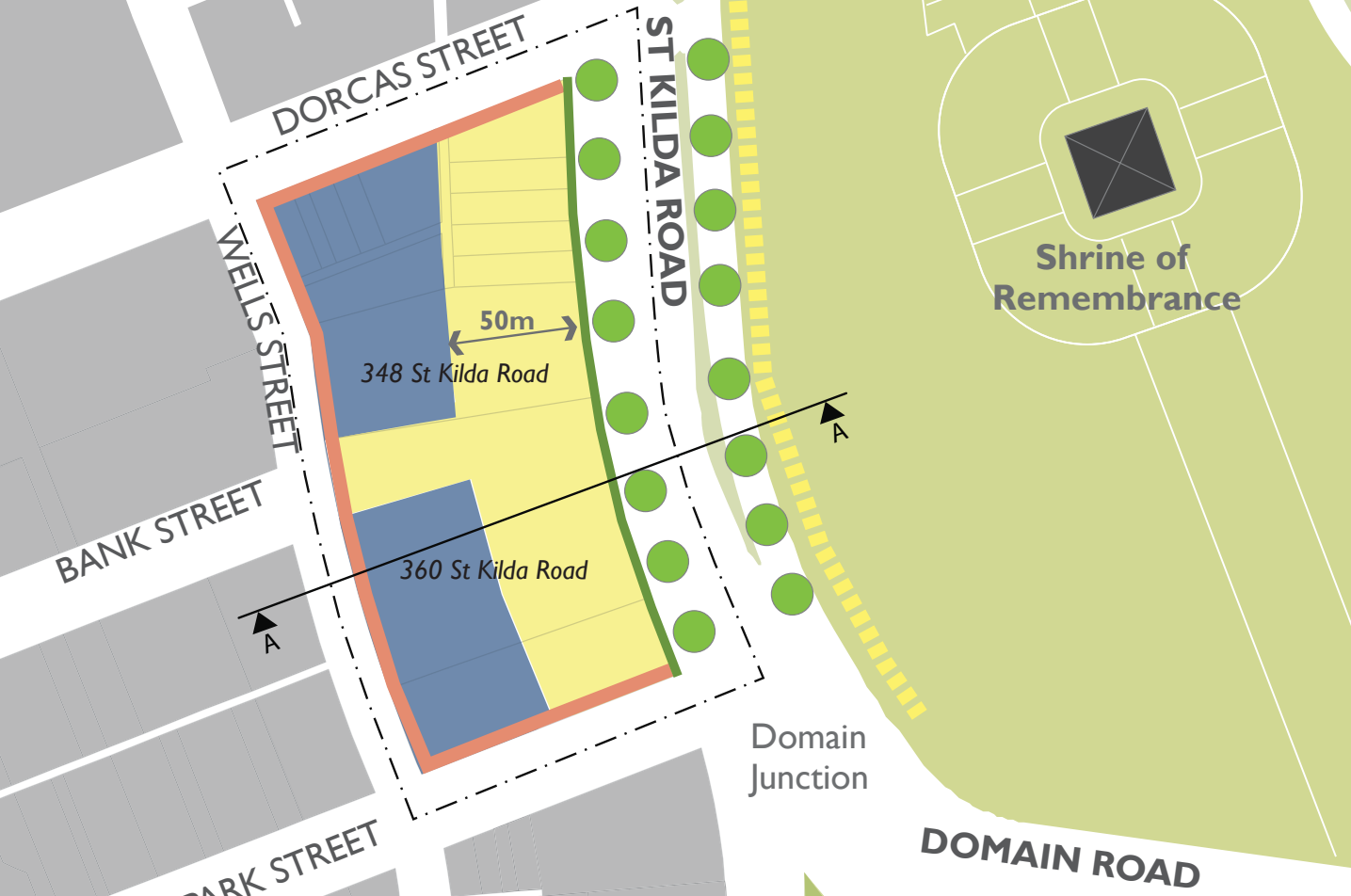
The Edge of Shrine Memorial Gardens is located in the northern end of the Precinct adjacent to the municipal boundary with the City of Melbourne. It is bounded by St Kilda Road, Park Street, Wells Street and Dorcas Street. The Sub-precinct forms the western backdrop to the Shrine of Remembrance and the edge to the Memorial Gardens and Domain Parklands.

The scale, form and design of buildings within this Sub-precinct are critical in respecting the sensitivity of the Shrine setting. Buildings will form a western silhouette to the Shrine.

In addition to the Shrine, the world renowned St Kilda Road boulevard with its leafy feel, substantial canopy trees and generous proportions is the other key feature of the Sub-precinct.

The built form character of St Kilda Road within Sub-precinct I, north of the Domain, is different to that south of the Domain. Buildings on this part of St Kilda Road generally present a low-rise street wall with towers set behind, creating a strong sense of consistency.

The Sub-precinct has a mixed architectural character within buildings of different eras, scale and presentation to the street. The variety of land uses includes both commercial and residential



MAP 10: SUB-PRECINCT I BUILT FORM CONTROLS

PRECINCT I

MANDATORY CONTROLS

- 3m landscape setback
- 36m AHD podium
- 18m podium (depth of 5m)
- 60m

PUBLIC REALM STRATEGIES

- Maintain and enhance landscape boulevard
- Observe solar access objectives for Shrine Memorial Gardens

ACTIONS: EDGE OF SHRINE MEMORIAL GARDENS

BUILT FORM

Refer to Map 10: Sub-Precinct I Built Form Controls & Sub-Precinct I Section AA for specific built form requirements

- Development must provide a three metre landscaped setback to the direct frontage or abuttal to St Kilda Road.
- Along Dorcas, Wells and Park Streets, a podium must not exceed 18 metres. Any additional storeys must be set back 5 metres.
- Along St Kilda Road, beyond the landscape setbacks development must not exceed a podium height of 36 metres AHD.
- Development on Wells Street (at the intersection of Bank Street) must not exceed 36 metres AHD.
- Towers must be setback 53 metres (inclusive of landscape setback) from St Kilda Road.
- Beyond these setback requirements, development must not exceed 60 metres.
- Development must maintain solar access to the Shrine's Memorial Gardens at all times of the day and

year.

- Above the podium level, development must be setback 4.5 metres from side and rear boundaries.
- New development must not protrude into the Shrine's silhouette above the level of the Portico roof when viewed from Birdwood Avenue.
- Development should ensure that the key view corridor along Park Street between the Domain and Eastern Reserve is enhanced through a high quality of built form and landscaping.

ACCESS AND MOVEMENT

- Improve the informal pedestrian crossing options along Wells Street by installing pedestrian island refuges.
- Install a signalised crossing at the intersection of Park Street, Wells Street and Palmerston Crescent to improve pedestrian connectivity and opportunities for traffic exiting Wells Street and Palmerston Crescent.

PUBLIC REALM

Work with the City of Melbourne to develop a cohesive streetscape design for Dorcas Street, taking design cues from the southern side of Dorcas Street, south of Wells Street.

RATIONALE: EDGE OF SHRINE MEMORIAL GARDENS

BUILT FORM

To protect the Shrine setting, the existing built form, with its consistent low scale deep podiums at street edge with high towers behind is to be reinforced. New development must be designed to respect the sensitivity of the Shrine setting within its immediate vicinity and from the more distant elevated points of the Shrine site.

Beyond the landscape setback, buildings along St Kilda Road must not exceed the mandatory 50 metre setback to a tower and podium height of 36 metre Australian Height Datum (AHD) (approximately 25 metres above the footpath level on St Kilda Road).

These mandatory controls reflect the existing podium / tower form in the Sub-precinct, and the podium height also aligns with the height of the Shrine's Portico.

The mandatory heights and extensive tower setbacks will ensure that development does not further intrude upon the silhouette of the Shrine, and that key view corridors are enhanced and framed by development and landscaping.

At present, two tall developments at 360 and 348 St Kilda Road tower above adjoining developments. These towers encroach into the Shrine's silhouette above the level of the Portico when viewed from its eastern elevation on Birdwood Avenue. Further encroachment into this silhouette is not supported due to the significance of the Shrine setting.

The mandatory 60 metre height limit above the deep podium will ensure that further tall buildings do not encroach on the Shrine's backdrop, and help to balance the composition currently created by the two towers on either side of Bank Street.

The 60 metre height limit and setbacks will also ensure that higher scale towers above the podium level appear recessive when viewed from the Shrine Portico. The substantial setback of the towers contributes to the sense of spaciousness around the Shrine setting. The podium form also relates well to the relatively low scale and squat form of the Victoria Barracks building.

The Plan will also protect the Memorial Gardens from overshadowing. The Gardens are used frequently for ceremonial purposes and require solar access at all times of the year.

At the Wells Street/Bank Street, intersection, a mandatory maximum height of 36 metre AHD will also be applied. Buildings will be kept low along this critical view corridor between the Shrine terrace and the South Melbourne Town Hall Clock Tower. The Shrine's axis is a critical element of the building's siting and design.

Along Dorcas, Wells and Parks Streets, a podium height of 18 metre with a five metre setback to tower will enhance the public realm and contribute to a network of pedestrian friendly streets. 18 metres is equivalent to five storeys which is considered to create a human scale at street level.

Pedestrian amenity, which is poor along parts of Park, Dorcas and Wells Streets will be enhanced by a maximum podium height of 18 metres creating a sense of enclosure and unity to streets. The 60 metre maximum height limit will also apply to these streets.

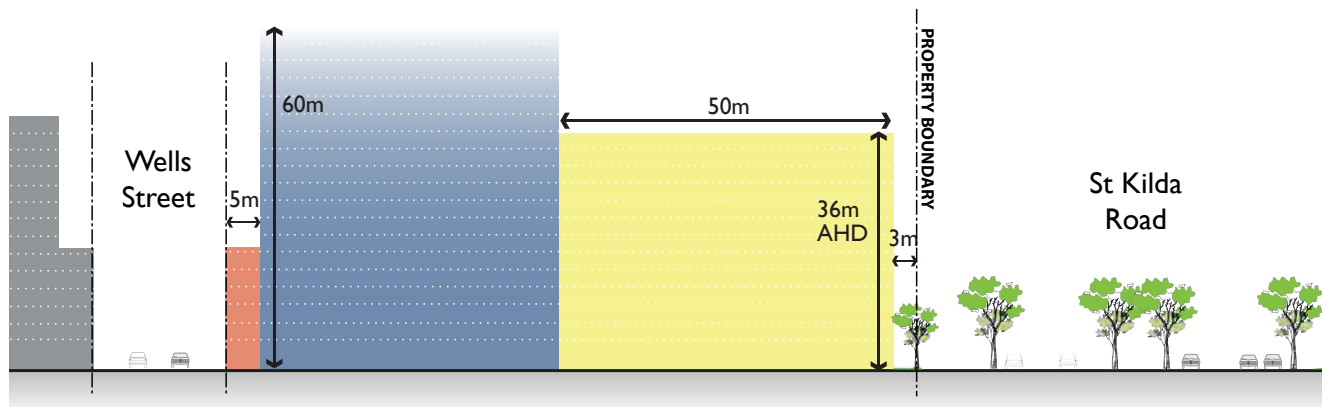
At ground level, active frontages such as building entrances and windows will be encouraged to enliven the streets, improve amenity and the perception of safety. Residential, commercial and mixed use development that includes a range of complementary active uses at ground level will also assist in creating a vibrant inner urban mixed use environment.

A five metre setback to the tower elements of new buildings will help to preserve sunlight and loss of sky views. It can also help in managing detrimental effects to public and private amenity such as overlooking, noise, light spill and extreme wind effects.

Mandatory tower separation above podium level of 4.5 metres setback from side and rear boundaries apply across the entire St Kilda Road North Precinct and will help to alleviate issues at street level around wind tunnelling and support solar access to apartments.

PUBLIC REALM

Co-ordination with the City of Melbourne to replicate the high quality streetscape appearance of the western end of Dorcas Street will enhance the overall look, feel and function of the shared streetscape.



Sub-Precinct I Section AA



4.2 SUB-PRECINCT 2

Northwest Corner



VISION: NORTHWEST CORNER

Sub-precinct 2 will be characterised by:

- *development of a vibrant residential and mixed use environment, in a higher scale and density of development*
- *a visual and physical graduation from the higher scale development of St Kilda Road, across the Sub-precinct to the low scale residential heritage area of South Melbourne (west of Kings Way)*
- *podium heights that contribute to a human scale along Kings Way and achieve a related and transitional built form on the two sides of Kings Way*
- *fine grain character reflected in the form and articulation of new buildings.*
- *active, high quality pedestrian environment at street level*
- *high quality public realm with additional tree planting and access to sunlight on key pedestrian streets (Bank and Park Streets)*
- *the development of Kings Way as a landscaped high quality boulevard with quality architectural design and public realm interface.*

DESCRIPTION

The northern boundary of Sub-precinct 2: Northwest Corner adjoins the higher density areas of Southbank along Dorcas Street in the City of Melbourne.

Palmerston Crescent forms the southern boundary, with Kings Way and the low scale heritage residential neighbourhood of South Melbourne form the western boundary. To the east, the Sub-precinct is generally bound by Wells Street.

Overall the Sub-precinct has a complex inner urban character, with diverse land uses and buildings typical of a former industrial / commercial area that has almost completed a transition into a residential / mixed use area. However, large format commercial or service businesses are clustered along or near Kings Way, largely as a response to its high exposure and poor amenity for residential development.

The larger land blocks between Bank and Dorcas Streets have generally all been recently developed into multi-storey residential apartments. Significant improvements have been made to the public realm through widened footpaths, grassed nature strips, canopy trees planted in regular spacing, reconfigured on-street car parking and in some cases, public art. A pedestrian link located mid-block is a recent inclusion that provides a convenient short-cut link for foot traffic and is an example of the pedestrian friendly environment sought for the St Kilda Road North Precinct.

The built form of relatively smaller land blocks and narrow streets south of Bank and Park Street has not changed dramatically in recent years. These lots are generally smaller in size and set to the street edge with very limited landscaping. There is significant scope to improve the public realm in this part of the Sub-precinct.



MAP II: SUB-PRECINCT 2 BUILT FORM CONTROLS

ACTIONS: NORTH WEST CORNER

BUILT FORM

Refer to Map 11 Sub-Precinct Built Form Controls & Sub Precinct Section AA for specific built form requirements

- A mandatory 3 metre landscaped setback must be provided to the direct frontage or abuttal to Kings Way
- Within 13m (inclusive of the landscape setback) of a Kings Way frontage or abuttal, development must not exceed 18 metres. Within 5 metres of all other street frontages or abutments, development must not exceed 18m.
- Beyond the above landscape/podium requirements, development must not exceed 45 metres in height.
- Development above the podium level must be setback 4.5 metres from side and rear boundaries.
- Building facades must follow the alignment of the street frontage to mimic the distinctive curvilinear street pattern.
- Development should reinforce the fine grain pattern of the Sub-precinct.
- Large redevelopment and proposals that consolidate smaller sites should incorporate through-block pedestrian links and fine grain elements into their design.
- Development will ensure that the key view corridor along Park Street between the Domain and Eastern Reserve are enhanced through a high quality of built form and landscaping.
- Development must maintain the existing levels of solar access to the southern footpaths of Bank and Park Streets when measured between

PRECINCT 2 MANDATORY CONTROLS

- 3m landscape setback
- 18m podium (refer Section AA for depth)
- 45m

PUBLIC REALM STRATEGIES

- Observe solar access objectives for southern footpaths of Bank and Park Street
- Potential new open space
- Streetscape upgrade
- Maintain and enhance landscape boulevard
- New landscape boulevard

10 am and 2 pm at the equinox.

- Development must maintain solar access to the adjoining residential areas south west of Kings Way and comply with the objectives of Clause 55.04-5 (overshadowing of open space objective) of the Port Phillip Planning Scheme.
- New development must provide visual modelling demonstrating the relationship of the proposal upon the Shrine setting.

PUBLIC REALM

- Investigate the opportunity to reconfigure the road space of Kings Place / Cobden Street to create a new open space as part of redevelopment of the Sub-precinct.
- Work with the City of Melbourne to develop a cohesive street scape design for Dorcas Street, taking design cues from the southern side of Dorcas Street, west of Wells Street.

ACCESS AND MOVEMENT

- Convert the intersection of Park Street, Wells Street Palmerston Crescent to traffic signals
- Design a continuous bike lane along Park Street.
- Work with Yarra Trams to improve the separation of tram operations from vehicle traffic
- Investigate opportunities to remove some of the vehicle parking at the intersection of Park Street/Kings Way to create additional queuing capacity for vehicles turning right.

RATIONALE: NORTH WEST CORNER

BUILT FORM

Sub-precinct 2: Northwest Corner will evolve into a higher density residential enclave, incorporating a mix of uses that complement lively, pedestrian focussed streets. The Sub-precinct is relatively unconstrained by heritage buildings or adjoining open spaces. The current mixed character and generally low level of street scape amenity also provides an opportunity for a new urban environment to form.

A maximum building height of 45 metres (mandatory) will create a stepping down in built form and a more distinct transition from the high rise buildings in St Kilda and Albert Roads and the traditional residential scale of the surrounding neighbourhood.

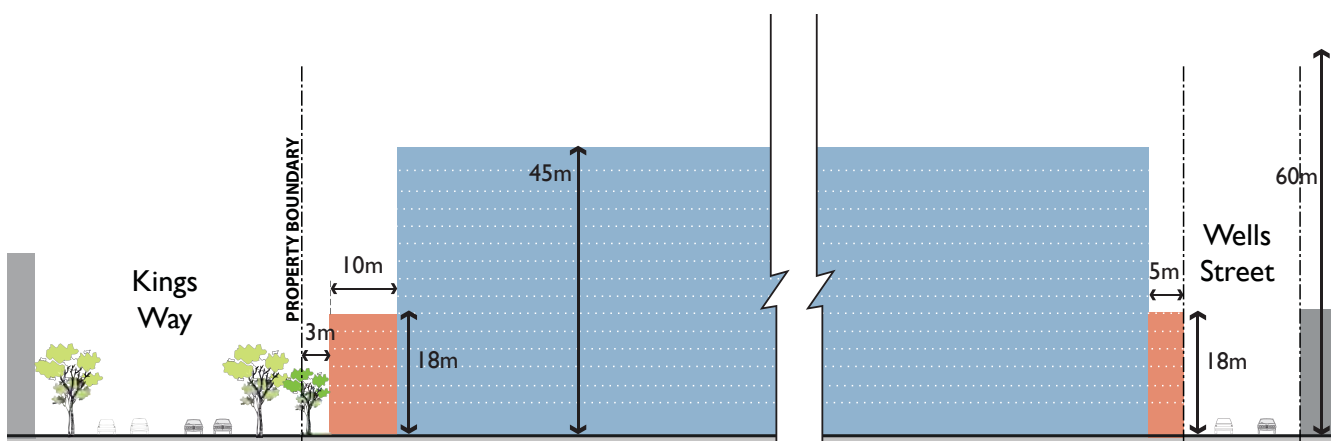
The application of a maximum height of 45 metres will also mediate the variation in heights between the two areas (high rise and traditional residential). The maximum height of 45 metres is also consistent with the heights of recent approvals and generally consistent with the scale of existing taller buildings across the precinct as a whole.

It is important to note that due to the elevated vantage point of the Shrine, higher scale buildings in this Sub-precinct will be visible when viewed from the Shrine terrace and may also intrude upon the Shrine's silhouette when viewed from its eastern elevation on Birdwood Avenue.

Proposed buildings will need to be modelled against the height datum of the Shrine to assess the impact upon the Shrine's setting and ensure that new development within the eastern view corridor does not intrude into the Shrine's silhouette above the portico roof. The Shrine Trustee's Built Form Review may place additional limits on building scale in this Sub-precinct.

A key direction of the plan is to develop Kings Way as a boulevard and better integrate St Kilda Road North Precinct with the rest of South Melbourne to the west of Kings Way. Traffic volumes, the poor quality public realm, and poor pedestrian permeability mean that Kings Way is a barrier that physically isolates, and limits integration of the precinct with the rest of South Melbourne. The quality of architectural design and improvements to the public realm interface by new development will make significant contributions to the improvement of Kings Way and improving this connection.

A maximum podium height of 18 metres with 5 metre setback requirement for towers above podiums will apply across the Sub-precinct (except for Kings Way). This 18m podium height requirement will also apply in other Sub-precincts as a unifying element of the Plan. This height will enhance the public realm and contribute to a network of pedestrian friendly streets.



Sub-Precinct 2 Section AA

The 18 metres podium height allows the equivalent of five storeys. This allows intensity of development and framing of streets whilst maintaining a human scale at street level. A fine example of this concept is The Elms development north of Dorcas Street.

The 18 metre podium also applies to Kings Way (with a 10 metre setback). Creating a human scale streetscape is the starting point for achieving the boulevard character of Kings Way and addressing factors which currently contribute to it forming a barrier. An 18 metre podium height will also achieve a related and transitional built form on the two sides of Kings Way and provide a transitional scale to the adjoining heritage areas.

A further consideration in applying the 18 metre podium is the width of Kings Way. The road reserve of Kings Way is approximately 15 metre less than St Kilda Road. (Kings Way is approximately 45 metres wide and St Kilda Road is approximately 60 metres wide.) 18m is in proportion to the width of the road.

The future liveability of the precinct is reliant on improved integration with the rest of South Melbourne to improve access to community services, retail facilities and transport. The height (and other works proposed in the plan) will also assist with the reintegration of the precinct with South Melbourne. This is particularly important given the projected increase in the residential population.

The 5 metre tower setback above podium is widely applied across St Kilda Road North Precinct creates a visual distinction between upper and lower levels of buildings. It can also help in managing detrimental effects to public and private amenity such as overlooking, sunlight access and extreme wind effects.

Mandatory tower separation above podium level of 4.5 metre setback from side and rear boundaries (applied across St Kilda Road North Precinct) will help to alleviate

issues at street level of wind tunnelling and support solar access to apartments.

A consistent mandatory 3 metre setback will specifically apply to properties fronting Kings Way to create a well-defined street edge. The landscaped street setback will provide space to plant canopy trees (on the footpath) to achieve the long term vision and develop Kings Way into a boulevard with a similar look and function to St Kilda Road. It will also assist in the short term by creating a buffer for building inhabitants and pedestrians to traffic on Kings Way.

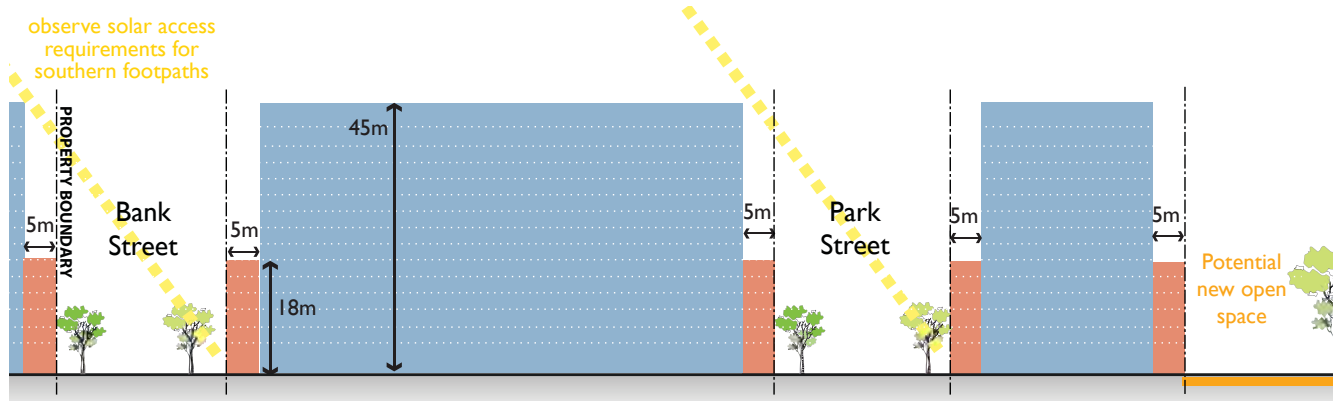
PUBLIC REALM

A number of specific public realm improvements are identified for Sub-precinct 2. Park, Dorcas, Bank and Wells Streets are key components of the local pedestrian network. The pedestrian amenity of these streets will be improved. In particular, the plan requires that solar access is maintained to the southern footpaths of Bank and Park Streets to ensure the opportunity for a new public places and use of footpaths is encouraged.

The aim is to further develop the network of pedestrian connections and creating a more intimate streetscape and vibrant residential and mixed use environment. Through-block pedestrian links and fine grain elements will be incorporated into the design of large redevelopment sites and where the consolidation of smaller lots is proposed.

Improving the Kings Way public realm and pedestrian permeability is vital to reconnecting the Precinct to the rest of South Melbourne. Future boulevard treatments and public realm improvements will improve the relationship between the built form on either side of the road.

The road space of Kings Place / Cobden Street also has the potential to be reconfigured to create a new open space if major redevelopment occurs.



Sub-Precinct 2 Section BB

ACCESS AND MOVEMENT

Park Street is an important east-west connection between the St Kilda Road corridor and the South Melbourne community across Kings Way. Crossing Park Street is currently difficult due to the amount of road space which is allocated to trams, and vehicles. To increase connectivity and improve accessibility, a formal pedestrian crossing should be installed at the intersection of Wells Street, Palmerston Crescent and Park Street. This will also help to improve the flow of vehicles out of and into Wells Street and Palmerston Crescent.

The plan also proposes to advocate to VicRoads and Yarra Trams to improve the operation of the Park Street tram. The operation of trams along Park Street can be affected during the day by vehicular congestion. Separating the tram tracks along Park Street and extend Route 55 west of Kings Way, or Route 1, east of Emerald Street, along Park Street so that it connects to South Melbourne and Clarendon Street will improve accessibility and connectivity.





4.3 SUB-PRECINCT 3

Albert Road South



VISION: ALBERT ROAD SOUTH

Sub-precinct 3: will be characterised by:

- A built form that is highest towards the Domain, gradually stepping down along Albert Road from Kings Way to Moray Street creating a transition in scale.*
- A medium rise built form on the Palmerston Crescent frontage providing a suitable transitional interface between Albert Park and Kings Way with adjoining heritage residential neighbourhood of South Melbourne.*
- Landscaped setbacks that enhance the leafy character of the area.*
- Active frontages which appeal to pedestrians and contribute to lively, attractive and safe streets.*
- Building facades which reflect the curvature of the underlying 19th century 'city beautiful' street pattern that is unique to the area.*
- Formal street planting that reflects the unique street pattern and is of a scale consistent with the podium levels.*

DESCRIPTION

Sub-precinct 3: Albert Road South is located between the established low scale residential areas of South Melbourne, Albert Park and the busy Kings Way. Buildings in Palmerston Crescent are substantially lower than those in Albert Road.

The Mac Robertson Girls School is located to the south of the Sub-precinct where Albert Road and Kings Way intersect.

The Sub-precinct has a highly varied subdivision pattern, distinctly different buildings scales, a range of lot sizes and variety of land uses. Buildings are of mixed quality and character. The area features commercial and high density residential uses similar to those in Sub-precinct 2 – east of Kings Way.



MAP 12: SUB-PRECINCT 3 BUILT FORM CONTROLS

PRECINCT 3

MANDATORY CONTROLS

- 3m landscape setback
- 18m podium (depth of 5m)
- 18-30m podium (depth of 10m)
- 9m

- 15m
- 18m
- 30m
- 45m
- 60m

PUBLIC REALM STRATEGIES

- Observe solar access objectives for MacRobertson's Girls High School

ACTIONS: ALBERT ROAD SOUTH

BUILT FORM

Refer to Map 12 Sub Precinct 3 Built form controls & Sub Precinct 3 Section AA for specific built form requirements

- A 3 metre landscaped setback must be provided to all street frontages, except laneways.
- Between Stead Street and Kings Way, development must not exceed 15 metres for the first 20 metres from Palmerston Crescent (inclusive of the 3 metre landscape setback), after which building height may increase to 30 metres up to a distance of 50 metres from the Palmerston Crescent frontage. Beyond this, heights may increase to 45 metres or 60 metres (Refer to Map 12 and Sub-Precinct 3 Section AA for specific requirements).
- Between Stead Street and Eastern Street, development must not exceed 15 metres for the first 20 metres from Palmerston Crescent (inclusive of the 3 metre landscape setback), after which development may increase to 30m.
- Between Eastern Road and Moray Street, development must not exceed 15 metres within 20 metres of Raglan Street and 9m within 20m of Moray Street (inclusive of the 3 metre landscape setback), after which development height may increase to 18 metres or 30 metres (Refer to Map 12 and Sub-Precinct 3 Section AA for specific requirements).
- Beyond the 15m setback requirement podium heights must:
 - be between 18 metres to 30 metres on Stead Street and Kings Way, with additional storeys setback 10 metres;
 - not exceed 18 metres on Sandilands Street and the eastern side of Eastern Road, with additional storeys setback 5 metres;
 - not exceed 18 metres on Thomason Street (eastern end) and the western side of Eastern Road, with additional storeys setback 5 metres;
- No podium is required for properties fronting Albert Road (corner sites should not express podium requirements to

Albert Road frontages)

- The following mandatory development heights apply (Refer to Map 12 for specific requirements):
 - Kings Way to Stead Street: 60 meters and 45 meters
 - Stead Street to Eastern Road: 30 meters
 - Eastern Road to Moray Street: 30 meters, 18 meters and 9 meters.
- Above the podium level, development must be setback 4.5 metres from side and rear boundaries.
- Development does not need to incorporate upper level setbacks to laneways in this sub-precinct.
- Development facades must follow the alignment of the road frontage to follow the distinctive curvilinear street pattern.
- New development must not result in any additional overshadowing of the Mac Robertson Girls' High School and Albert Park at June solstice between 11am and 2pm.
- Development at boundaries with properties within a residential zone must comply with the objectives of Clause 55.04-5 (overshadowing of open space objective) of the Port Phillip Planning Scheme.

ACCESS AND MOVEMENT

Work with VicRoads to:

- Install a bike lane along the entire length of Albert Road.
- Upgrade the signalised intersection of Kings Way and Albert Road to improve the conditions for pedestrians by increasing the amount of time for the crossing phase and to provide a green light for bikes phase.

RATIONALE: ALBERT ROAD SOUTH

BUILT FORM

Sub-precinct 3 clearly demonstrates elements of the historic City Beautiful movement in city planning with the parallel and curved forms of Palmerston Crescent and Albert Road. Buildings will be designed to reflect the curvature of the streets creating consistency and rhythm in the built form and contributing to a coherent built form 'character' for the Sub-precinct.

A mandatory 3 metre landscaped setback will be required in all streets in the Sub-precinct and will help establish a leafy character to the area and defined edge to Albert Park.

A key objective of the plan is to achieve a stepped down built form along Albert Road and Palmerston Crescent. Building heights along Albert Road will gradually step in height from the higher scale buildings (punctuation mark) at the Domain to the two storey heritage buildings on Moray Street.

Stepping down also creates a positive interface with Albert Park to the south, particularly when viewed a distance from across the park.

Buildings at the corner of Albert Road and Kings Way will be up to 60 metres in height. This will create an effective transition from the higher scale buildings in the adjoining Sub-precincts to the north and east.

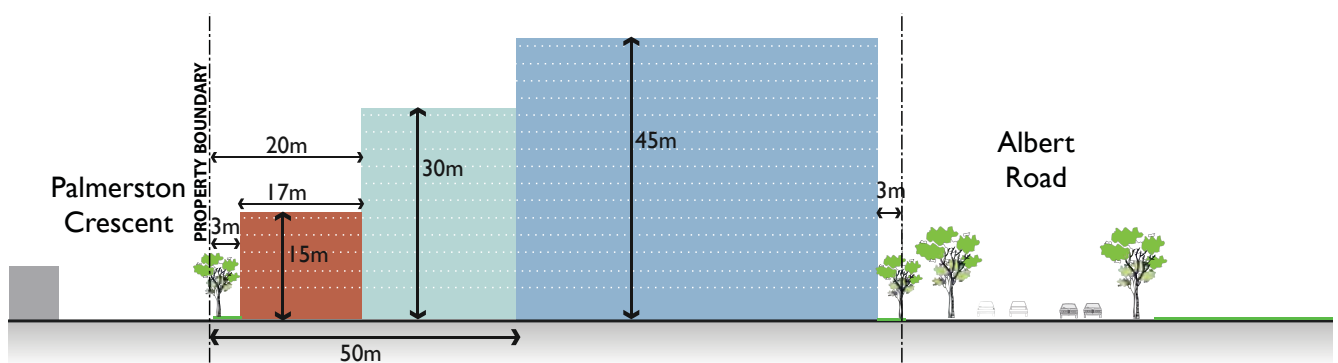
No podiums are required along Albert Road itself. The service road on Albert Road, established canopy trees and lack of podiums in the existing buildings combine to create a defined edge to Albert Park and a human scale to the street without the need for a podium.

However along other frontages, a podium form (except on Albert Road) will be required and will assist in creating a human scale. A mandatory 30m podium applies in Stead Street and along Kings Way reflecting the overall increase in building scale closer to Kings Way. A lower mandatory podium height of 18 metres applies to the lower scale areas of Sandilands Street, Eastern Road and Thomson Street.

Above the podium, towers will be setback 5 metres to preserve sunlight and maintain an outlook. It will also help to manage overlooking and extreme wind effects.

Towers will also be required to provide adequate separation side and rear boundaries. This will help alleviate issues at street level such as wind tunnelling, support solar access to apartments and maximise privacy. Maximum tower widths also apply.

Building height will also step down from Albert Road through block to Palmerston Crescent to provide a further transition to the adjoining residential areas along the north side of Palmerston Crescent. A medium rise scale of up 4 storeys (15 metres) is sought on the southern side of Palmerston Crescent. (This height is marginally higher than existing discretionary height control of 12 metres, however



Sub-Precinct 3 Section AA

the existing upper level setback of 7 metres to higher built form is increased to 17 metres.)

A consistent frontage height of 15 metres along Palmerston Crescent (including 3 metres ground level setback) with an increased upper level setback of 17 metres to the higher scale form behind will reduce the visual impact of these higher scale buildings from the street and ensures amenity considerations are adequately taken into account.

The increased height would allow the renewal of aged buildings or underused sites while maintaining an acceptable scale at the street frontage opposite the low scale heritage residential area. The height is also appropriate given the distinct roles of the different sides of Palmerston Crescent (i.e. residential versus a more commercial role).

ACCESS AND MOVEMENT

Albert Road is an arterial road managed by VicRoads. It performs a key function of connecting other north-south arterial roads in the precinct and at peak times is dominated by vehicle traffic which makes it unappealing to pedestrians and bike riders. Providing facilities which prioritise pedestrians and bike riders is a key to encouraging the uptake of these more sustainable transport choices. Extension of a bike route down the full length of Albert Road will enhance bike riding options in the precinct, improve safety and enhance connectivity.

PUBLIC REALM

The majority of streets within sub-precinct 3 typically feature consistent footpaths, grassed nature strips and a regular rhythm of canopy trees. These elements, along with consistent building setbacks, contribute to an overall impression of a high quality public realm and should be retained and enhanced.

Minor streetscape improvements on Thomson Street would be a beneficial contribution to the overall leafy character of the area.







4.4 SUB-PRECINCT 4

Albert Road North & Bowen Crescent

VISION: ALBERT ROAD NORTH AND BOWEN CRESCENT

Sub-precinct 4 will be characterised by:

- Its role as the focal point of the wider St Kilda Road North Precinct with higher scale high quality buildings where Albert Road, St Kilda Road and Domain Road meet.*
- Distinctive street pattern where building frontages mimic the curve of the street and are complemented by formal street tree planting.*
- Buildings with a podium form to create a human scale at ground level.*
- Active, high quality pedestrian environment at street level.*
- Its continued development as a vibrant inner urban residential and mixed use area.*
- New buildings which provide new high quality living spaces for a growing residential population and protect enhance the amenity of existing residential developments and adjoining residential neighbourhoods in South Melbourne.*
- Improved public streets with additional street trees and increased pedestrian connections.*

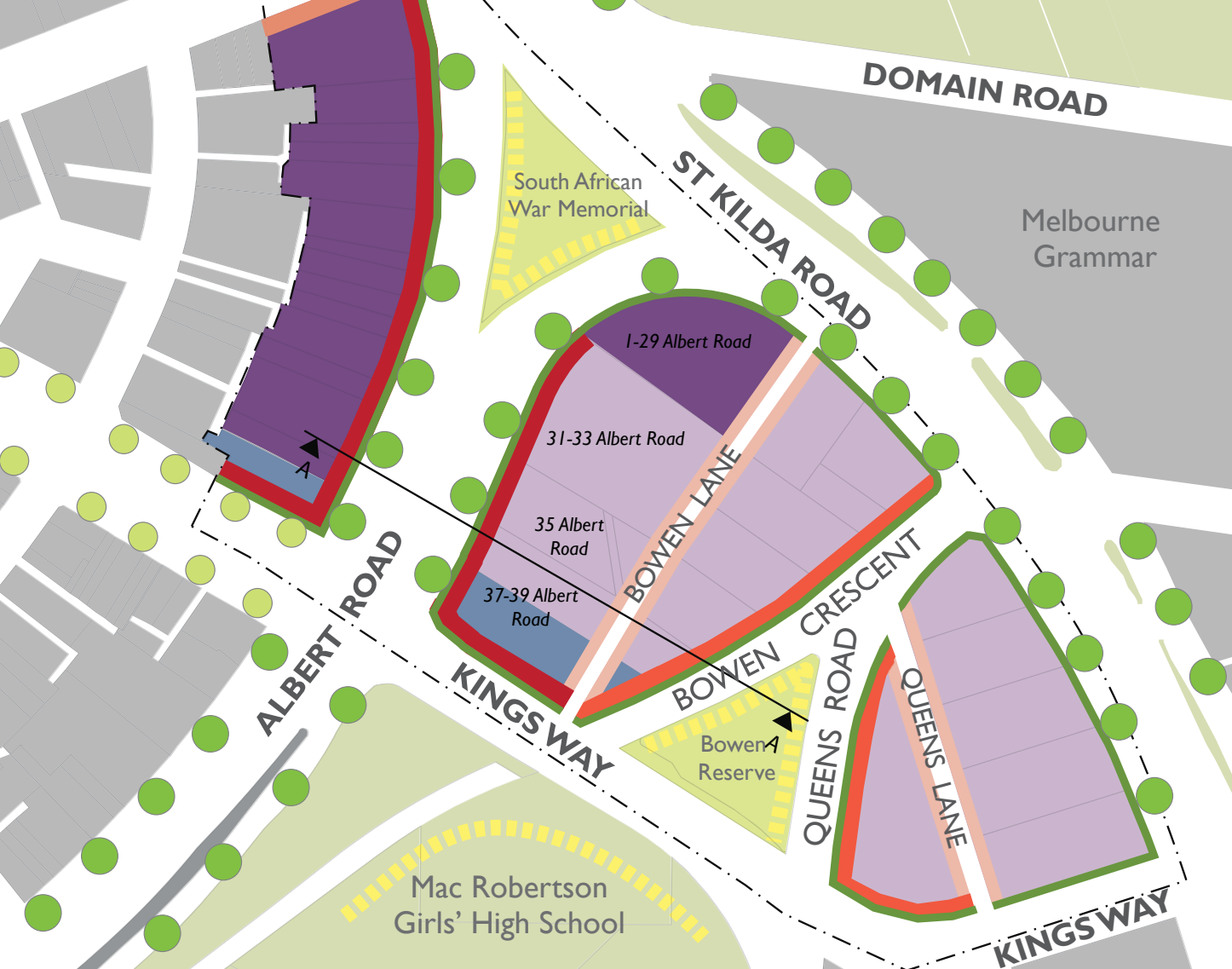
DESCRIPTION

Sub-precinct 4: Albert Road North & Bowen Crescent is located where St Kilda Road and Albert Road, as it curves around the elevated ground of the Domain and the Shrine of Remembrance, converge. Kings Way and Queens Road also meet within Sub-precinct 4. Sub-precinct 4 forms an important point of transition between Sub-precincts to the north and south.

This Sub-precinct is distinctive in its curvilinear road layout, wide street proportions and landscaped pocket parks – key characteristics of the 19th century City Beautiful movement.

Major roads and public transport corridors converge in this Sub-precinct. It is also bordered by open grounds of Mac Robertson Girl's High School, Melbourne Grammar and the southern edge of the Shrine Reserve/ Domain Junction.

Sub-precinct 4 has a mixture of high density residential and office uses and includes a number of historically significant buildings. It also has some of the highest buildings in the St Kilda Precinct located within it. These buildings, including the heritage listed former BP House (1-29 Albert Road), form distinctive focal points.



MAP 13: SUB-PRECINCT 4 BUILT FORM CONTROLS

ACTIONS: ALBERT ROAD NORTH AND BOWEN CRESCENT

BUILT FORM

Refer to Map 13 Sub-Precinct 4 Built Form Controls & Sub-precinct 4 Section AA for specific built form requirements

- Development must provide a mandatory 3 metre landscaped setback to all street frontages (except Bowen Lane, Queens Lane and Park Street).
- Development must provide an:
 - 18 metre to 30 metre podium to Albert Road (excluding 1-29 Albert Road) and Kings Way, north of Bowen Lane, with any additional storeys set back 10 metres..
 - 18 metre to 30 metre podium to the north side of Bowen Crescent, Queens Road and Kings Way (west of Queens Lane) with additional storeys setback 5 metres.
 - 18 metre podium to Park Street with additional storeys setback 5 metres.
- Development fronting Bowen Lane or Queens Lane must
 - be built to the Bowen Lane or Queens Lane boundary; and
 - within 5 metres of Bowen Lane or Queens Lane not exceed a height of 11 metres.
- Corner sites should not express podium requirements to St Kilda Road frontage.

PRECINCT 4 MANDATORY CONTROLS

- 3m landscape setback
- 11m podium (depth of 5m)
- 18m podium (depth of 5m)
- 18m - 30m podium (depth of 5m)
- 18m - 30m podium (depth of 10m)
- 60m
- 75m
- 90m

PUBLIC REALM STRATEGIES

- Observe solar access objectives for open space and MacRobertson's Girls High School
- Maintain and enhance landscape boulevard
- New landscape boulevard

- Development along the north/west side of Albert Road (excluding 70 Albert Road), as well as I-29 Albert Road must not exceed 90 metres in height.
- Development at 70 Albert Road, and between Albert Road and Bowen Crescent within 30 metres of Kings Way must not exceed 60m in height
- Beyond the above requirements, development must not exceed 75 metres.
- Above the podium level, buildings must be setback 4.5 metres from side and rear boundaries.
- Building facades must align with the street frontage
- New development must not result in any additional overshadowing of the:
 - South African War Memorial Reserve and the Bowen Crescent Reserve at the equinox between 10am and 2pm.
 - Mac Robertson Girls' High School and Albert Park at June solstice between 11am and 2pm.

ACCESS AND MOVEMENT

- Work with VicRoads to install a bike lane along Albert Road providing a safe connection to St Kilda Road.

RATIONALE: ALBERT ROAD NORTH AND BOWEN CRESCENT

BUILT FORM

Building heights will step down from 90 metres at the Domain to 60 metres at the intersection of Albert Road and Kings Way.

A strong built form edge of a high architectural standard is essential to frame key view corridors from the Shrine

along Albert Road to towards the Bay and along Bowen Crescent. To achieve this, a consistency of ground floor and upper level setbacks is essential.

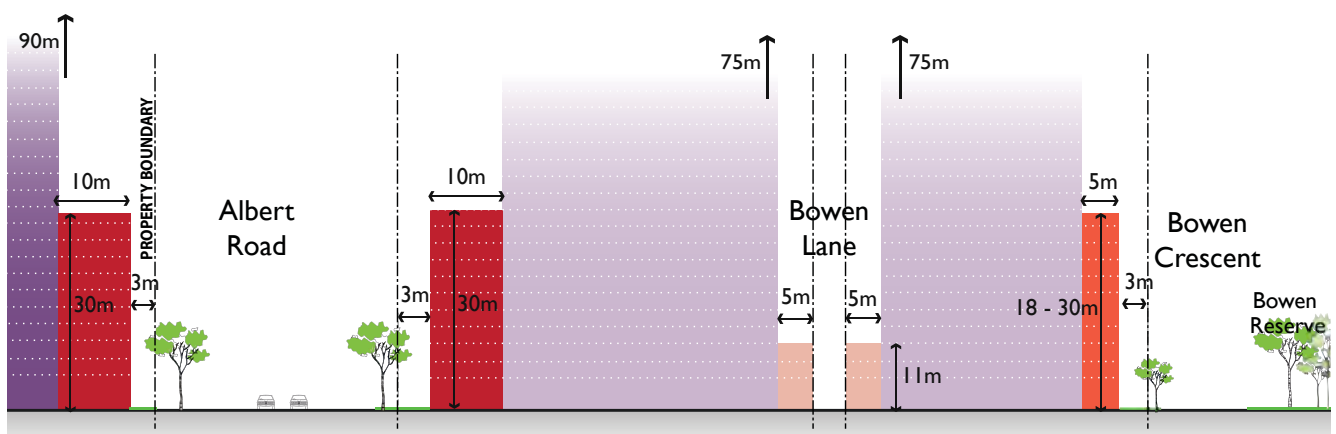
Mandatory heights of 90m (at I-29 Albert Road), 75m (at 31-33 and 35 Albert Road) and 60m (at 37-39 Albert Road) will achieve an appropriate transition in the scale of buildings between St Kilda Road through to Kings Way/Queens Road, when viewed from Albert Park towards the Domain Junction.

A cluster of higher scale buildings around the Domain will help to emphasise the change in road alignment in the St Kilda Road axis as it curves around the elevated ground of the Domain and the Shrine, and as the point of transition between Kings Way and Queens Road. 90 metres reflects the height of existing approved development in the Sub-precinct and to the north-west and is appropriate given the generous width of the road space.

A lower scale of development (60 metres) at the corners of Albert Road and Kings Way will assist in creating a transition from the South African War Memorial and the Domain from Albert Park and help to frame this view. The mandatory overall height limits of 90, 75 and 60 metres will provide significant residential development opportunities while still maintaining the key built form character of the Sub-precinct. It will also facilitate a range of complementary active uses at ground level.

Buildings will be designed in a tower / podium form to create a human scale at ground level and will provide active edges to the street. A mandatory podium height of between 18 and 30 metres (approximately 5 to 8 storeys) will be applied to achieve this. The podium height responds to the generous width of Albert Road and will also help to frame view corridors.

Towers will be setback by 10 metres where buildings are



Sub-Precinct 4 Section AA

located on Albert Road and 5 metres to Bowen Crescent and Queens Road. A 10 metre setback to the tower will maintain key view corridor from the Shrine to the Bay. The 5 metre setback ensures that the tower is visually recessive.

New buildings in Queens Lane and Bowen Lane will have zero ground level setback with a 3 storey (11m) podium to enhance the role of these lanes as pedestrian connections and encourage natural surveillance.

Mandatory 3 metre landscaped street setbacks provide an attractive and enclosing street edge along Bowen Crescent Reserve and Albert Road. It will establish a leafy character to the area, provide better definition to the edges of Albert Park along Queens Road, and reinforce a network of pedestrian connections throughout the Sub-precinct.

Building frontages that reflect the curvature of the distinctive street pattern of the Sub-precinct will create consistency and rhythm in the built form, contributing to a coherent built form 'character' for the Sub-precinct.

Mandatory tower separation of 4.5 metre setback from side and rear boundaries above podium level will reduce impacts such as wind tunnelling at street level and loss of light to building inhabitants.

ACCESS AND MOVEMENT

To encourage the uptake of bike-riding, people have to feel comfortable within the road environment and welcomed by the infrastructure provided by it. The installation of bike lanes are a useful device in indicating the bike-riders are welcome and it aids in their safe use of the road through providing space away from vehicles in which to travel. An existing gap in the bike network is the route along Albert Road. Installation of a bike lane along Albert Road will connect St Kilda Road and Albert Park / South Melbourne.

PUBLIC REALM

Sub-precinct 4 has established street planting, generous pedestrian areas and a network of small lanes. These elements typically result in an overall impression of a high quality, highly functional public realm; however in this sub-precinct the majority of streetscapes are utilitarian in appearance with building edges typically presenting blank walls to the footpath, and laneways that provide a largely service, rather than pedestrian, function.

Improvements can be made to significantly enhance the public realm of this sub-precinct. These include requirements for new private development to present continuous 'active' frontages to streets, including along laneways and thoroughfares and, where necessary, incorporate landscaped areas into building design that reinforces the geometry of the street pattern and complements the street character to the south (sub-precincts 5 & 6).







4.5 SUB-PRECINCT 5

St Kilda Road South of Kings Way



VISION: ST KILDA ROAD, SOUTH OF KINGS WAY

Sub-precinct 5 will be characterised by:

- The retention and strengthening of the highly valued boulevard character of St Kilda Road.*
- Consistent and symmetrical building heights on both sides of St Kilda Road.*
- Regularly placed buildings, with space between them that frame view corridors.*
- Generous landscaped front setbacks to St Kilda Road that contribute to the leafy boulevard character.*
- Landscaped links along east-west streets.*
- Active frontages at street level.*
- A continuation of its complementary roles as the premier office location outside of the Melbourne CAD and a highly desirable residential location.*
- Development of a high quality, high amenity mixed use area on Raleigh and Union Streets.*

DESCRIPTION

Sub-precinct 5: St Kilda Road, South of Kings Way extends along St Kilda Road from Toorak Road in the north to the junction with Queens Way in the south. It shares a number of municipal boundaries including City of Melbourne on the eastern side of St Kilda Road north of High Street and City of Stonnington to the east of Punt Road.

It has a different character to the area, north of the Domain with buildings on either side of the road. The consistency of building scale on either side of the road and established tree canopy make it a highly unique street scape in inner Melbourne. Taller buildings line the St Kilda Road corridor and taper in scale towards Queens Road and Punt Road.

West of St Kilda Road, the prevailing street grid and regular subdivision pattern that exists to the north is also a key character element.

The eastern wedge of this Sub-precinct around Raleigh and Union Streets has a different character. This area contains irregular shaped parcels and serves as a transition to the established residential setting of Windsor – further east of Punt Road.



MAP 14: SUB-PRECINCT 5 BUILT FORM CONTROLS

ACTIONS: ST KILDA ROAD SOUTH OF KINGS WAY

BUILT FORM

Refer to Map 14 Sub-Precinct Controls & Sub precinct 4 Section AA for specific built form requirements

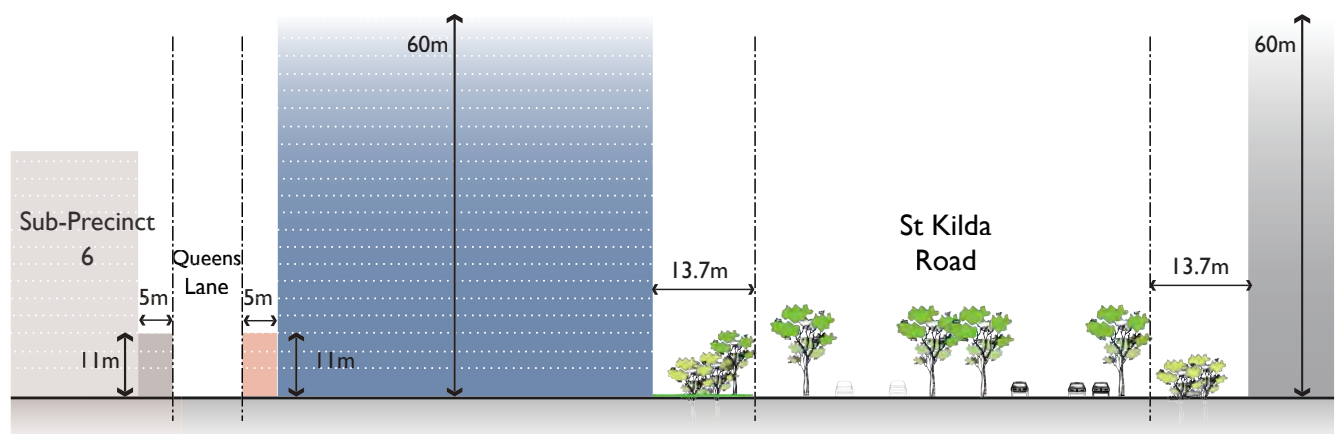
- Development must provide a mandatory landscaped setback to St Kilda Road frontages of 13.7 metres, to Queens Road of 6 metres, to Kings Way of 3 metres and to Arthur Street, Leopold Street, Louise Street, Hanna Street, Roy Street, Beatrice Street, Lorne Street, Union Street, High Street, Raleigh Street Henry Street and Punt Road of 4.5 metres
- Development must be setback 4.5 metres from the side boundaries.
- Development with a direct frontage or abuttal to Punt Road, High Street, Raleigh Street, Union Street and Henry Street must have a podium of 18 metres with a 5 metre setback to upper levels. Corner sites should not express this podium to St Kilda Road.
- Development fronting and abutting Queens Lane must:
 - be built to the Queens Lane boundary; and
 - within 5 metres of Queens Lane not exceed a height of 11 metres.
- Development must not exceed: 60 metres in height except buildings fronting Punt Road between Union and Raleigh Streets where buildings must not exceed 40 metres within 50 metres of Punt Road (including the landscape and podium requirements identified above.)
- Tower widths should not exceed a width and depth of

35 metres.

- Buildings must not result in additional overshadowing of Wesley College Junior School will between 11am-2pm at the June solstice.

ACCESS AND MOVEMENT

- Raised side road entry treatments should be installed to slow vehicles turning from St Kilda Road onto side streets and improve the safety and priority of pedestrians travelling along St Kilda Road.
- Council must work with VicRoads and the City of Melbourne to:
 - Provide separated and widened bike lanes along the length of St Kilda Road.
 - Improve pedestrian priority to cross St Kilda Road by upgrading traffic signals to improve green man time and provide auto call-up.
 - Improve pedestrian crossing conditions at the intersection of Queens Lane and Lorne Street and Queens Lane and Union Street through streetscape improvements that raise the profile and priority of pedestrians.



Sub-Precinct 5 Section AA

RATIONALE: ST KILDA ROAD SOUTH OF KINGS WAY

BUILT FORM

A consistent mandatory height of 60 metres will be applied to the length of St Kilda Road within this Sub-precinct. The mandatory height is essential to reinforce the distinct character of St Kilda Road which is defined by the consistency and symmetry of building heights on both sides of the road. This symmetry creates a sense of enclosure and balance. Isolated buildings above 60 metres would disrupt and diminish this clearly defined and highly valued characteristic. This height is consistent with the 60 metre height limit that applies to land in the City of Melbourne.

The proposed 60 metres height limit represents an increase in building heights for buildings south of Albert Reserve and south of High Street on the eastern side of St Kilda. This increase recognises the heights of recent developments throughout the precinct that have exceeded the 45 metre height limit of the current DDO4-3. Building heights on the eastern side of St Kilda Road will be subject to Design and Development Overlay 13 (DDO13) which protects the silhouette of the Shrine and would still apply.

A 40 metre height limit for properties fronting Punt Road between Union and Raleigh Streets applies to reflect the existing low scale development along Punt Road and recognising adjoining sensitive uses such as a school and a health care facility. A 40 metre height limit achieves a transition between the 60 metre area along St Kilda Road to the lower scale neighbourhoods east of Punt Road.

Podiums are not required along St Kilda Road. This reflects the role of a strong built form edge in framing the key view corridor. The absence of podiums along St Kilda Road will reinforce the generous boulevard character and symmetry of the precinct.

A podium of 18 metres on buildings with frontages to Punt Road and the connecting streets will assist in creating a more human scale at street level given that buildings could reach up to 60 metres in height in this location.

Mandatory side setback requirements will ensure a high degree of internal amenity for building occupants and ensure new buildings reflect the regular building spacing that is a characteristic of the Sub-precinct. Towers must also be separated to provide privacy and outlook for residents.

Regular building placement along St Kilda Road will also frame the view corridors within this long, linear boulevard.

Activity at the ground level of buildings and within the front setbacks will contribute to activity and amenity of the street scape. The landscaping of these spaces, as well as within the smaller frontage setbacks of buildings within the side streets of the Sub-precinct, will contribute to the leafy boulevard character of St Kilda Road.

A 13.7metre mandatory landscape setback will apply along

St Kilda Road and is reflective of existing development along the street. Application of consistent frontage setbacks with detailed landscape guidelines will enhance key view corridors along St Kilda Road between the Domain and St Kilda Junction.

Landscape guidelines will be developed that will encourage low or no front fencing. This will contribute to consistency in setback treatments and be complementary to the whole Sub-precinct.

Strengthening the landscape links along the streets connecting St Kilda Road and Queens Road is also important in this Sub-precinct as it is not serviced by any smaller or pocket parks. High quality landscape links along the side streets will help to link the major open spaces of Albert Park and Fawkner Park. To reinforce these links, all buildings will be set back 4.5 metres from their side boundaries (both shared boundaries and setbacks to side streets).

Detailed controls will also apply to Queens Lane to ensure quality frontages to this lane. Improving Queens Lane as a pedestrian link with buildings designed to address and enhance this space will help foster a quality walking environment that will be particularly attractive to residents.

This will include limiting building heights directly fronting the lane itself to three storeys or 11 metres with buildings setback by five metres above this height. Buildings will be permitted to be built hard up to the footpath. This will create a more intimate scale to the lane, improve the pedestrian environment and encourage natural surveillance. Consideration will also be given to better accommodating services in the lane (see Strategic Direction 3.5 Requirements: Detailed Building Design).

ACCESS AND MOVEMENT

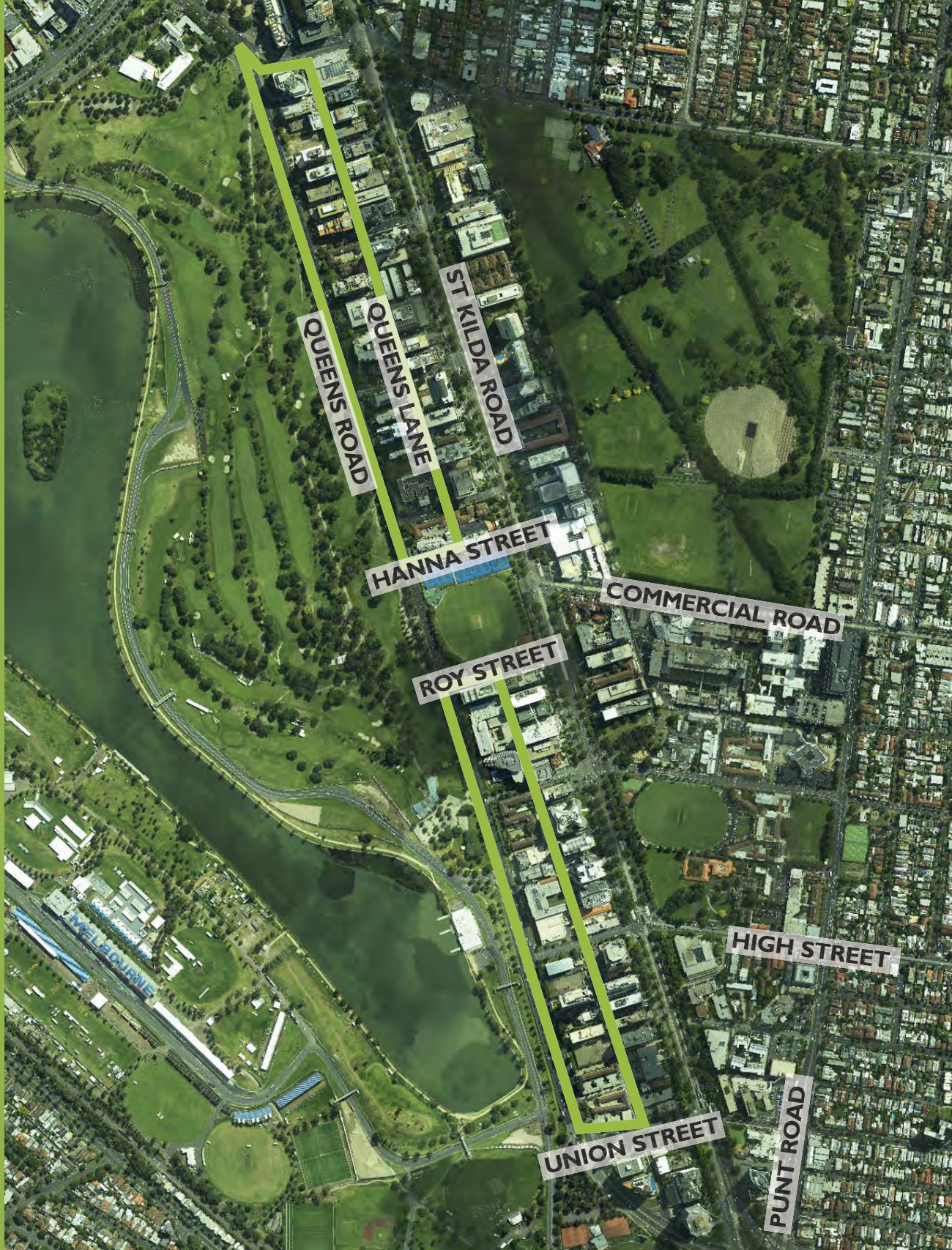
The road widths of Union Street and Lorne Street are particularly wide which makes pedestrian crossing difficult. Reducing the width of the streets will make it easier for pedestrians to cross and also improve the safety of the road by slowing vehicle speeds.

PUBLIC REALM

The majority of streets in sub-precinct 5 generally reflect a quality public realm. The east-west streets between Queens Road and St Kilda Road typically feature wide footpaths, grassed nature strips and regular rhythm of street trees. Landscaped forecourts combine with the beautiful canopy of mature trees, and regular spacing along the grand boulevard of St Kilda Road giving this sub-precinct a distinctive and high quality streetscape character.

Minor streetscape improvements such as additional landscaping and water sensitive urban design could utilise the extensive road-space within east-west streets and also strengthen better visual links between Albert Park Lake Reserve and Fawkner Park





4.6 SUB-PRECINCT 6

Queens Road



VISION: QUEENS ROAD

Sub-precinct 6: will be characterised by:

- *Generous and consistent front setbacks and regular spacing between buildings*
- *Buildings of a medium scale with towers setback above a podium*
- *Development which frames long ranging views along Queens Road and forms an edge to Albert Park*
- *High quality planting along the streets as well as canopy trees and other larger plants in the front setback to enhance the street scape and pedestrian space.*
- *The presence of important heritage buildings which will be retained and enhanced.*
- *Queens Lane will be improved as an important pedestrian link with buildings designed to address and enhance this space.*

DESCRIPTION

Sub-precinct 6: Queens Road is bordered by Albert Park along Queens Road, between Kings Way and Union Street to the west. Queens Lane forms the eastern boundary to the Sub-precinct and provides service and vehicle access to properties fronting Queens Road.

It has a mix of architecture styles with many individual heritage sites ranging from Victorian era mansions to mid twentieth century flats. It is distinct as a medium rise area with generous and consistent front setbacks, spaces between buildings and medium scaled heights with upper level setbacks are key elements of the street which frame long ranging views along Queens Road and define the urban edge to Albert Park.

There is a mix of office and residential use north of Arthur Street with predominantly residential uses south of Arthur Street.



MAP 15: SUB-PRECINCT 6 BUILT FORM CONTROLS

ACTIONS: QUEENS ROAD

BUILT FORM

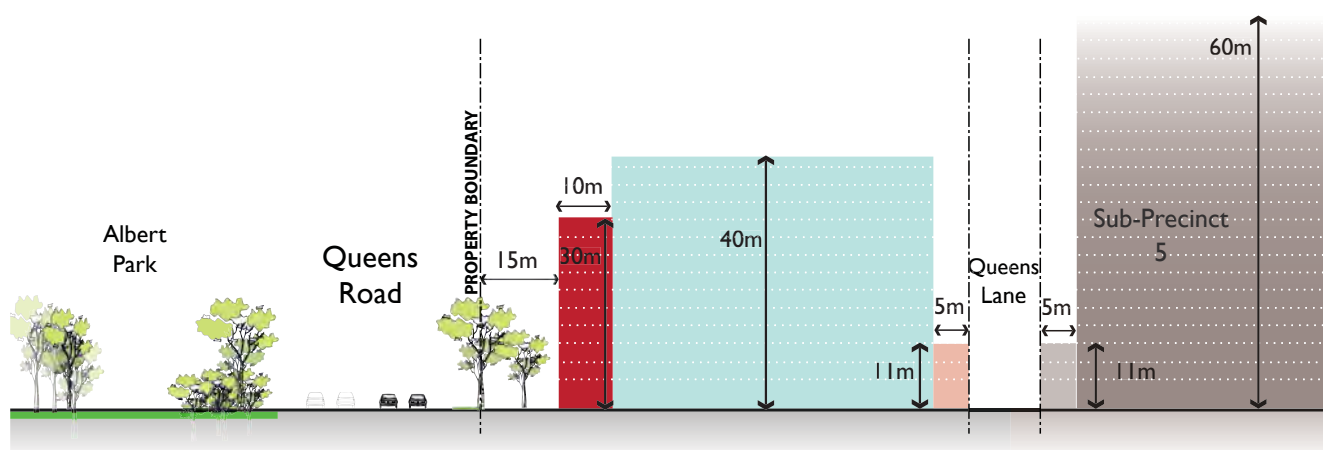
Refer to Map 15 Sub-Precinct Controls & Sub precinct 4 Section AA for specific built form requirements

- Development must provide a mandatory landscaped setback to St Kilda Road frontages of 13.7 metres, to Queens Road of 6 metres, Kings Way of 3 metres and to Arthur Street, Leopold Street, Louise Street, Hanna Street, Roy Street, Beatrice Street, Lorne Street and Union Street of 4.5 metres
- Development fronting and abutting Queens Lane must:
 - be built to the Queens Lane boundary; and
 - within 5 metres of Queens Lane not exceed a height of 11 metres.
- South of Arthur Street, development within 25 metres (inclusive of the 15 metre landscape setback) of any street frontage to Queens Road must not exceed 30m. Development beyond the landscape setbacks and Queens Road and Queens Lane podium height limits must not exceed a height of 40 metres.
- North of Arthur Street, development beyond the landscape setbacks and Queens Lane podium height limits must not exceed 60 metres in height.

- Development must be constructed 4.5 metres from all side boundaries.

PUBLIC REALM

- Waste collection and loading for developments along Queens Lane should occur on-site.
- Widen the footpaths and underground power lines on Queens Lane to ensure footpath widths meet minimum requirements.
- Council should work with VicRoads to improve the connectivity for pedestrians and bike riders by:
 - Upgrading the intersection of Queens Road and Lorne Street to provide increased green man time and auto call-up and provide a formal pedestrian crossing on all legs of the intersection.
 - Improving the crossing from Albert Park into Union Street through the provision of bike lights within the traffic signal system.



Sub-Precinct 6 Section AA

RATIONALE: QUEENS ROAD

BUILT FORM

The existing character of Sub-precinct 6 – Queens Road will be maintained and enhanced through a consistent approach to building heights, spacing between buildings, front setbacks and landscaping.

Building heights in the Sub-precinct currently vary along the length of Queens Road and a number of height controls apply in the planning scheme. Buildings are higher at either end of Queens Road (ie near Kings Way and the St Kilda Junction). A 60 metre mandatory height limit, north of Arthur Street and south of Union Street (in Sub-precinct 5), 'bookends' the sub-precinct.

However between Arthur and Union Streets, buildings are lower. To reinforce the mid-rise scale, a mandatory building height of 40 metres will be applied in this stretch of the road. This will provide a consistent height across the Precinct and provide a definitive building edge to the Sub-precinct when viewed from across Albert Park.

The lower scale height, compared to St Kilda Road is in part a response to the many heritage buildings in this area. Heritage buildings within the street scape will be retained and enhanced as valuable reminders of the area's past as a favoured location for grand mansions, institutional buildings and early flat developments.

A further key characteristic of Queens Road that the plan will reinforce is the podium / tower style of buildings. Many existing buildings have stepped upper levels. This element will be reinforced through the application of a 30 metre podium height (with 10 metre setback to any towers) that will apply along the length of Queens Road (south of Arthur Street.)

Buildings will also be setback 4.5 metres from each other at ground level to assist in strengthening the landscape links and overall 'garden' character of the Precinct.

A generous front setback of 15 metres will be applied along Queens Road to maintain a consistent landscaped frontage (and reflecting the existing built form controls for area). The front setback will be mandatory to reinforce this is an essential element of the street scape character. It will also form a landscaped edge to the Park and emphasis the Queens Road vista.

Consistent, high quality planting along the street and in private setbacks will enhance the street scape and pedestrian space of Queens Road and strengthen the landscape interface with Albert Park.

To enhance landscaping in the Queens Road Sub-precinct and the broader Precinct, Council will prepare a public realm strategy and detailed landscape guidelines. The guidelines for the Queens Road Sub-precinct will set out setback landscaping requirements, including encouraging the planting of taller, canopy trees and low or no front fencing.

Strengthening the landscape links along the streets connecting St Kilda Road and Queens Road to connect the major open spaces of Albert Park and Fawkner Park is also important in this Sub-precinct.

PUBLIC REALM

Queens Road is dominated by vehicle use, has inconsistent footpath widths and high fencing and narrow verges. As a result, relatively few pedestrians choose Queens Road as a walking route and the options for a pedestrian crossing are limited.

Council will work in conjunction with VicRoads to improve connections for the residents of the St Kilda Precinct through improvements to the operation of the pedestrian signals at Queens Road/Union Street and Queens Road/Lorne Street.

Improving the amenity of Queens Lane and further developing it as an attractive pedestrian link by encouraging building designs to address and enhance the lane, will provide an excellent option for people to walk locally. Queens Lane will continue to provide building services or vehicular access for properties fronting Queens Road and St Kilda Road, but these will be designed in a manner that recognises its function as an important pedestrian connection.



**CLIVE FAIRBAIRN
PAVILION**

ST KILDA ROAD NORTH PRECINCT 2013

05 Implementation Framework

Implementation of the St Kilda Road North Plan requires an integrated approach, led by Council with the cooperation and sharing responsibility with the State Government, private sector and community stakeholders.

The recommendations of the Plan will be implemented via a number of statutory and non-statutory mechanisms, including:

- Amending the Port Phillip Planning Scheme – A new statutory planning framework will be developed to implement the strategic directions of the Plan. Planning Scheme policy and provisions will manage development proposals from the private sector.
- Council Capital Works, Programs and Services - Council's 5 Year Capital Works Plan and Council services such as Sustainable Transport and Children's and Family Services.
- Partnerships - with State Government agencies, land owners / developers and community organisations.

KEY PARTNERSHIPS

Implementation of the Plan will require Council to work in partnership with key stakeholders including:

- City of Melbourne and Shrine Trustees to ensure consistency with related projects and strategic objectives of the Shrine.
- Department of Transport, Public Transport Victoria and Yarra Trams – public transport improvements, including advocating for the Metro One and Park Street tram extensions.
- VicRoads – provision of bike lanes, traffic management and street tree planting along key transport corridors.
- City of Melbourne - provision and access to community services.

PUBLIC REALM

1 Year	1-5 Year	5 Year +
STRATEGY 1 Maintain St Kilda Road as Melbourne's premier boulevard		
<p>Advocacy Work with City of Melbourne to develop principles for the maintenance and upgrade of street scapes to ensure a consistent management approach to trees and nature strips along both sides of St Kilda Road.</p> <p>Design Incorporate Water Sensitive Urban Design interventions into footpath and road design.</p>	<p>Design Prepare an improvement works plan for surfaces and street furniture.</p> <p>Carry out works to improve grass cover and provide alternatives to hard surfaces, for example the installation of sub-surface irrigation</p>	
STRATEGY 2 Redevelop Kings Way as an attractive tree-lined street		
<p>Advocacy Work with VicRoads to implement street beautification opportunities.</p> <p>Design Incorporate Water Sensitive Urban Design interventions into footpath and road design.</p> <p>Policy Use Planning provisions to require extensive at-grade car parking areas located within street setbacks incorporate varying levels of planting (grasses, shrubs and trees) to enhance the appearance of the street scape and conceal parking areas.</p> <p>Research Investigate opportunities to provide for multiple users including pedestrians, bike riders, public transport and traffic.</p>	<p>Advocacy Work with Yarra trams, VicRoads and City of Melbourne to explore the potential to 'green' the image of the street scape utilising the central tram line space and maximise landscaping opportunities within private development setbacks to both sides of the road.</p> <p>Design Improve the existing footpath surface by applying consistent high quality finishes.</p>	<p>Design Plant large canopy trees along both sides of the street and in the median (if possible).</p>
STRATEGY 3 Reinforce the boulevard character of Albert Road		
	<p>Policy Implement the Bike Plan 20011 by constructing an on-road bike lane.</p> <p>Design Prepare detailed landscape design guidelines to introduce a formal planting theme to reinforce the geometry of the street layout and extend a 'green link' between the Shrine and Albert Park.</p> <p>Prepare detailed landscape design guidelines to ensure the street scape is complemented with quality landscaping effects such as forecourts and with quality paving treatments.</p> <p>Prepare detailed landscape design guidelines that describe a preferred diverse range of planting.</p>	

1 Year	1-5 Year	5 Year +
STRATEGY 4 Reinforce the boulevard character of Queens Road		
<p>Advocacy</p> <p>Advocate to Parks Victoria for improvements to the existing chain wire fencing along the Albert Park golf course with a new fence using higher quality materials that will retain views into the parkland and maintain a sense of 'openness'.</p>		
STRATEGY 5 Enhance the role of St Kilda Road North Precincts streets in the open space network		
<p>Design</p> <p>Prepare detailed landscape design guidelines to guide the strengthening of 'green' visual links on east-west aligned streets between Albert Park and Fawkner Park.</p> <p>Standardise footpath, nature strip treatment and tree pit designs across all streets.</p> <p>Prepare detailed landscape design guidelines to reduce clutter by co-locating street furniture.</p> <p>Investigate an integrated pedestrian plaza as part of any new development on the island site/s off Cobden Street, south of Kings Place.</p> <p>Policy</p> <p>Use Planning provisions to require that ground level setbacks are consistently landscaped with tall trees, allowing views to the building façade and across larger front setback areas.</p> <p>Encourage developments with St Kilda Road frontages to design forecourts with public seating so they can be used for public, private and communal use.</p>	<p>Policy</p> <p>Implement the <i>Open Space Strategy 2009</i></p> <p>Implement the <i>Greening Port Phillip, An Urban Forest Approach 2010</i></p> <p>Advocacy</p> <p>Work with City of Melbourne to achieve a cohesive street scape design along Dorcas Street, taking design cues from the southern side of Dorcas Street – south of Wells Street.</p> <p>Design</p> <p>The public realm where possible to collect, treat and reuse rainwater and stormwater.</p> <p>Design the public realm to minimise the need for and improve the efficiency of irrigation through the use of porous/permeable surfaces on roads and footpaths, the use of drought tolerant vegetation and efficient irrigation systems, and through the placement of bioretention street trees that are supplied by stormwater and therefore do not require ongoing irrigation.</p>	<p>Design</p> <p>Major street scape upgrade to Park Street between St Kilda Road and Ferrars Street in South Melbourne to strengthen this physical and visual link to the Shrine Reserve.</p> <p>Bank Street landscaping improvements to include new footpaths, grassed nature strips, street tree planting and Water Sensitive Urban Design (WSUD) planting bays within the street to reduce the extent of hard surfaces and strengthen this unique visual link between the Shrine of Remembrance monument to the east and the South Melbourne Town Hall to the west.</p>
STRATEGY 6 Capitalise on opportunities associated with new development to create new public spaces to address the needs of the existing and future local community		
<p>Design</p> <p>Design an integrated pedestrian plaza as part of any new development on the island site/s off Cobden Street, south of Kings Place.</p> <p>Policy</p> <p>Encourage developments with St Kilda Road frontages to design forecourts with public seating so they can be used for public, private and communal use.</p>	<p>Design</p> <p>Purposefully design new public spaces as part of proposed street scape upgrades.</p>	

1 Year	1-5 Year	5 Year +
STRATEGY 7 Integrate urban art to contribute to the community's positive experience of public space		
Policy Require publicly-accessible frontages to incorporate lighting, seating and public art to contribute to a safe and interesting pedestrian environment. Require new development to incorporate urban art that reflects the Precinct's identity and community values.		

TRANSPORT AND ACCESS

1 Year	1-5 Year	5 Year +
STRATEGY 1 Deliver high quality, integrated public transport		
Advocacy Continue to advocate for the development of a rail station at the Domain. Work with the Department of Transport, Yarra Trams and VicRoads to ensure all tram stops along St Kilda Road comply with the Disability Discrimination Act (DDA), display real time information, and provide a high level of amenity (including shelter) for users.	Advocacy Advocate for an extension to the Park Street tram line to Clarendon Street to connect with South Melbourne and Docklands. Work with the Department of Transport, Public Transport Victoria, bus providers and VicRoads to improve accessibility of buses, including the potential for shared use of tram stops for bus services. Design Implement left-in/left-out restrictions on minor road approaches to provide greater priorities to trams in St Kilda Road North Precinct.	
STRATEGY 2 Expand and upgrade cycling and walking networks		
Policy Ensure off-street loading facilities are provided in the following developments: <ul style="list-style-type: none"> - All large multi-storey residential developments. (Facilities should be designed to cater for Medium Rigid Truck in accordance with AS 2890.2.2002.) - All mixed use and commercial developments (comply with Planning Scheme Clause 52.07). 	Policy Implement the <i>City of Port Phillip Bike Plan 2011–2020</i> <i>Pedal Power: Making bike riding better</i> by upgrading and extending the existing on- and off -road cycle paths to form a comprehensive and safe, well signed network, linking shops, public transport and activity nodes. Install a new bike path on the South side of Albert Road.	Design In conjunction with VicRoads install pedestrian operated signals mid-block on Park Street. Construct new crossings and central island pedestrian refuges to enable people to cross Park Street at/near Wells Street/ Palmerston Crescent conveniently and safely.

1 Year	1-5 Year	5 Year +
STRATEGY 2 (continued)		
<p>Policy (cont)</p> <p>Require large development sites to integrate new pedestrian linkages in design and construction.</p> <p>Require new developments to remove existing crossovers along St Kilda Road and Kings Way.</p> <p>Require new residential and commercial developments to provide end-of-trip facilities such as bike racks, secure storage, shelters and change rooms at the following:</p> <ul style="list-style-type: none"> - bicycle parking for building occupants and visitors on every site - all-day parking at trip destinations (e.g. for employees and students) - all-day/part-day parking at public transport stations or interchanges - short-term parking at shops, offices and other institutions. <p>Advocacy</p> <p>Work with VicRoads and City of Melbourne to improve the key commuter bike route along St Kilda Road by:</p> <ul style="list-style-type: none"> - Construct a separated bike path the length of St Kilda Road to separate bike riders from parked and moving vehicles. - Introduce joint priority at traffic signals with pedestrians and trams. - Install bike hoops along the St Kilda Road verge and within private forecourts and buildings for workers and visitors. - Request Vic Roads down grade the traffic function of St Kilda Road as an arterial road for motor vehicles. 	<p>Policy (cont)</p> <p>Provide bike facilities along Kings Way (east end), Dorcas Street and High Street.</p> <p>Research</p> <p>Investigate opportunities for a new pedestrian and cycle crossing of Queens Road to connect the Precinct with Albert Park.</p> <p>Review the traffic management of Queens Lane where it intersects with Arthur, Leopold, Louise, Beatrice and Lorne Street.</p> <p>Review waste bin collection times to avoid conflict with higher pedestrian flows during the peak periods and ensure new developments deal with waste collection on-site.</p> <p>Design</p> <p>Upgrade the pedestrian environment in St Kilda Road North Precinct to provide safe direct access to all public transport services.</p> <p>As part of the <i>City of Port Phillip Walk Plan 2011–2020</i>, develop a comprehensive, well-connected and safe pedestrian network throughout St Kilda Road North Precinct that links open space, (new) urban plazas/forecourts, and public transport with residential areas and work places.</p> <p>Improve waste collection along Queens Lane to remove bins at pedestrian peak usage times.</p>	

1 Year	1-5 Year	5 Year +
STRATEGY 2 (continued)		
<p>Design Maximise the enjoyment and comfort of walking, with extended footpaths, extensive tree planting, quality street furniture and lighting, and active street frontages across the Precinct.</p> <p>Research Audit and upgrade all pedestrian facilities (especially kerb ramps at intersections) for DDA compliance. Audit and upgrade footpath surface along St Kilda Road, especially where construction/repairs have occurred.</p>		
STRATEGY 3 Efficiently manage traffic movements through and to the area		
<p>Policy Require the provision of a minimum of one bicycle parking space per dwelling for all new residential development in St Kilda Road North Precinct. Require loading be done off-street as loading on St Kilda Road can interfere with the safe operation of the bike lane.</p>	<p>Research Prepare a Sustainable Parking Precinct Plan that specifies a low (and maximum) rate for residential and office developments and no requirements for visitor parking.</p>	

URBAN STRUCTURE AND BUILT FORM

1 Year	1-5 Year	5 Year +
STRATEGY 1 Create a vibrant neighbourhood precinct		
<p>Policy Prepare a planning scheme amendment to implement revised built form controls. This include urban design initiatives such as pedestrian entrances that are a key feature of the façade, open directly to the street, and are at the same level as the public footpath.</p>		
STRATEGY 2 Develop built form controls that create a high amenity, high quality walkable environment		
<p>Policy Prepare a planning scheme amendment to implement revised built form controls.</p>		

<i>1 Year</i>	<i>1-5 Year</i>	<i>5 Year +</i>
STRATEGY 3 Integrate new development with character and scale of adjacent suburbs		
Policy Prepare a planning scheme amendment to implement the revised built form controls		
STRATEGY 4 Create high quality, liveable dwellings that include housing choice		
Policy Require all new residential and non residential developments, alterations and additions to achieve Green Star rating of 6 star or equivalent. Utilise the Sustainable Design Assessment in the Planning Process (SDAPP) framework to assess planning permit applications.	Design Develop landscaping guidelines to improve the quality and quantity of private open spaces, including the implementation of green roofs, walls and façades in new developments. Integrate these guidelines into the planning scheme to ensure development applications meet these guidelines.	
STRATEGY 5 Create new public places through building design		
Policy Include requirements for the activation of public places into the Planning Scheme Amendment.		

COMMUNITY INFRASTRUCTURE

<i>1 Year</i>	<i>1-5 Year</i>	<i>5 Year +</i>
STRATEGY 1 Establish a St Kilda Road North Precinct Community Centre		
Advocacy Establish and continue partnerships with City of Melbourne and relevant institutions and organisations for the shared provision of community services.	Research Conduct a feasibility study for the development of a community hub in the Precinct. This feasibility will include assessing the capacity of existing neighbouring facilities and services and how they can be integrated and/or redeveloped into a community hub model.	

SUSTAINABLE INFRASTRUCTURE

1 Year	1-5 Years	5+ Years
STRATEGY 1 Construct efficient buildings		
<p>Policy</p> <p>Implement policy for energy, water and waste efficiency of buildings through Port Phillip Planning Scheme Amendment C97.</p> <p>Implement best practice passive design principles including orientation, shading, natural daylight and natural ventilation when designing and siting buildings.</p>	<p>Advocate</p> <p>The placement of green roofs, walls or facades to reduce the urban heat island effect and improve local biodiversity, air quality and stormwater generated from the site.</p>	
STRATEGY 2 Reduce impact of flooding		
<p>Design</p> <p>Use water sensitive urban design (WSUD) to capture and treat rainwater and stormwater for reuse on site or for controlled discharge back to the environment.</p>	<p>Design</p> <p>Improve drainage infrastructure, install detention systems, and enable diversion to established storage systems for reuse or controlled release of water, to significantly reduce the loadings on the drainage system after heavy rains.</p> <p>Research</p> <p>Prepare a feasibility study in partnership with key stakeholders, to consider where the placement of local sustainable water management infrastructure within the precinct, including third pipe, sewer mining, and distributed storage systems.</p>	
STRATEGY 3 Incorporate opportunities for sustainable infrastructure into any street upgrades.		
<p>Design</p> <p>Design to collect, treat and reuse rainwater and stormwater. The use of measures such as rainwater tanks, vegetated swales, raingardens, and directing the flow from impervious ground surfaces to landscaped areas can reduce the pressure on drainage infrastructure and mitigate overland flooding after heavy rains.</p>	<p>Design</p> <p>Design porous/permeable surfaces on roads and footpaths, use of drought tolerant vegetation and efficient irrigation systems, and place bioretention street trees that are supplied by stormwater and therefore do not require ongoing irrigation.</p> <p>Research</p> <p>Seek opportunities to connect to local sustainable water management solutions providing treated stormwater or blackwater.</p>	

<i>1 Year</i>	<i>1-5 Years</i>	<i>5+ Years</i>
STRATEGY 4 IMPLEMENT WASTE MANAGEMENT IMPROVEMENTS		
<p>Policy</p> <p>Incorporate controls for the off-street storage of waste materials and services, fully screened from areas of high pedestrian activity, into the Planning Scheme Amendment.</p> <p>Require waste management plans to be prepared for all new developments covering construction and operational waste.</p>	<p>Research</p> <p>Review the times of waste collection to seek to reduce conflict between collection trucks and peak hour pedestrian and transport movement.</p>	



For more information, please call ASSIST on 03 9209 6777

Facsimile: 03 9536 2722

SMS: 0432 005 405

or email: assist@portphillip.vic.gov.au

National Relay Service Assistance:

24 hour TTY / Voice: 133 677

Speak and Listen: 1300 555 727

You can also visit our website: www.portphillip.vic.gov.au

Postal address:

City of Port Phillip

Private Bag 3, PO St Kilda, Vic 3182

For a translation of this information, please contact the Council's interpreter service.

中文请拨 03 9679 9810

Polski 03 9679 9812

Other Languages 03 9679 9814

Κλήσεις στα ελληνικά 03 9679 9811

На русском языке 03 9679 9813
– ЗВОНИТЕ ПО ТЕЛ

Please contact ASSIST on 03 9209 6777 if you require
a large print version of this brochure.